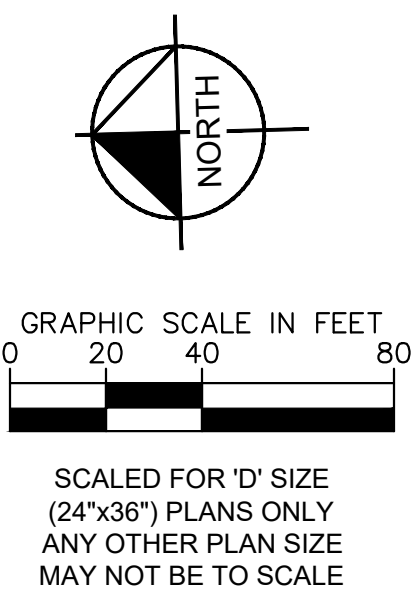
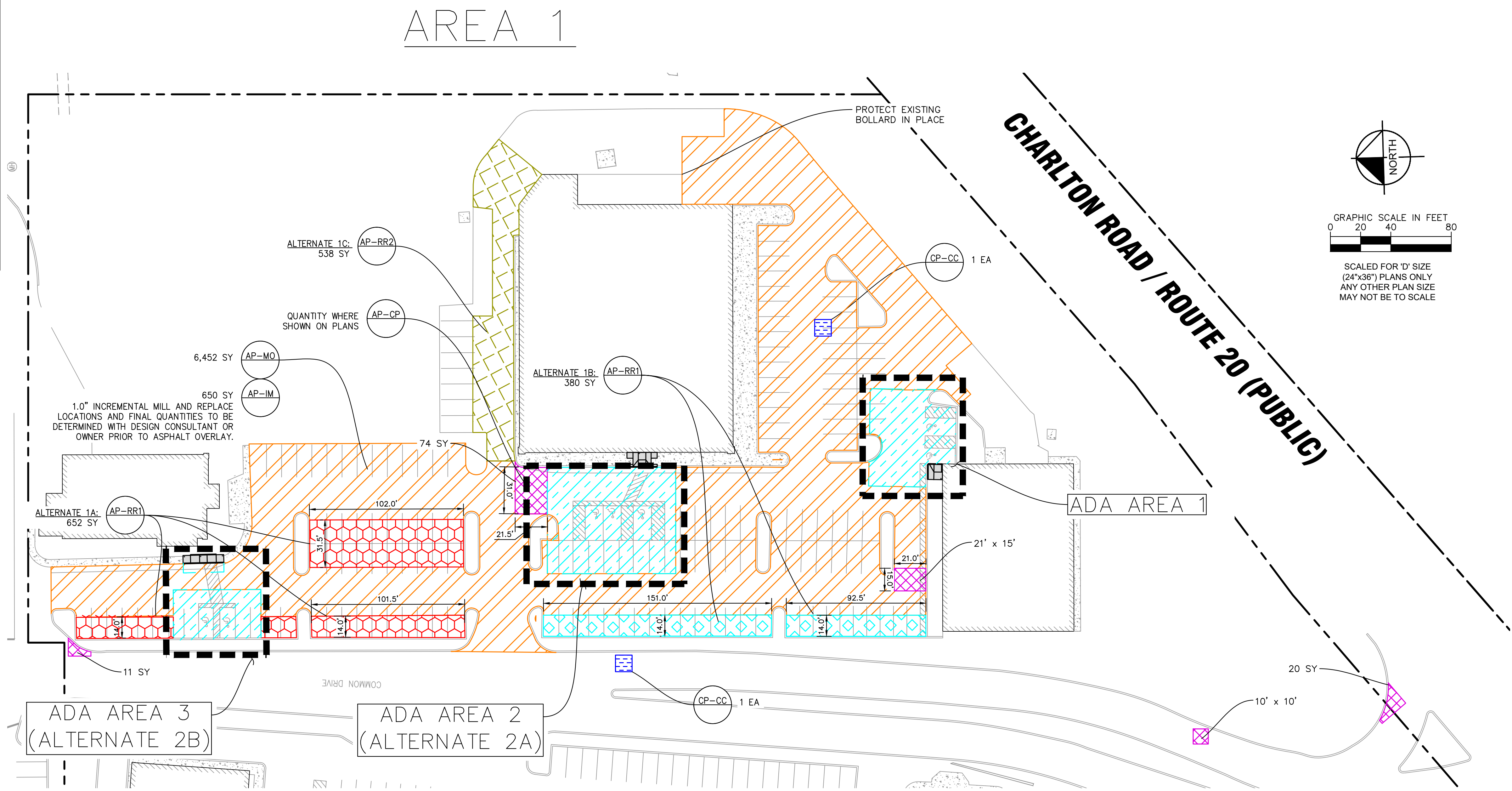


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PROPOSED PAVING IMPROVEMENTS	
	DETAIL REFERENCE (SEE APPROPRIATE DETAIL SHEET)
ASPHALT IMPROVEMENTS	
	CRACK SEAL
	2.0" MILL AND OVERLAY
	CUT AND PATCH
	REMOVE, REGRADE, AND REPLACE
	REMOVE, REGRADE, AND REPLACE LIMITS
	ALTERNATE A: 2.5" REMOVE AND REPLACE
	ALTERNATE B: 2.5" REMOVE AND REPLACE
	ALTERNATE C: 3.5" REMOVE AND REPLACE
	ALTERNATE D: CRACK SEAL
CONCRETE IMPROVEMENTS	
	CONCRETE COLLAR
	REMOVE, REGRADE, AND REPLACE SIDEWALK
	LEVEL LANDING (MAX. 2.0% SLOPE IN ALL DIRECTIONS)
	REMOVE AND REPLACE VERTICAL CURB
MISCELLANEOUS IMPROVEMENTS	
	ADA SIGN AND POST WITH BOLLARD
	REVISED STRIPING
	ADA SYMBOL WITH BACKGROUND
	REGRADE AND RESTORE LANDSCAPING



NOTE: INCIDENTAL CONSTRUCTION TO BE INCLUDED IN PAVING IMPROVEMENT LUMP SUM PRICES SHALL INCLUDE SOIL EROSION & SEDIMENTATION CONTROL LANDSCAPING RESTORATION, TEMPORARILY REMOVING AND RESETTING CONCRETE BLOCKS OR OTHER PORTABLE DEVICES, REPLACEMENT OF PAVEMENT STRIPING AND PAVEMENT MARKINGS, STRUCTURE ADJUSTMENTS, PERMITS, AND PROVIDING BARRIER-FREE DOCUMENTATION OF COMPLETED WORK.

APPROXIMATE LOCATIONS OF INCIDENTAL CONSTRUCTION ARE SHOWN WHERE APPLICABLE. QUANTITY MAY INCREASE OR DECREASE WHERE FOUND NECESSARY UPON SITE REVIEW WITH DESIGN CONSULTANT OR OWNER.

THIS PLAN SET IS BASED ON AUTOCAD FILES OBTAINED BY KIMLEY-HORN AND ASSOCIATES, INC. FROM CIM GROUP

DRAWING RECEIVED ON: JULY 27, 2016
 THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE EXISTING INFORMATION SHOWN ON THESE PLANS IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR WILL BE RESPONSIBLE FOR FIELD VERIFYING EXISTING SITE CONDITIONS BEFORE COMMENCING CONSTRUCTION. THE CONTRACTOR MUST ALSO CALL 811 AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES.



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Kimley-Horn					
SITE IMPROVEMENTS					
THE CENTER AT HOBBS BROOK STURBRIDGE, MA					
SHEET NUMBER C1-00					
EROSION CONTROL	NO.	REVISIONS	DATE		
1					

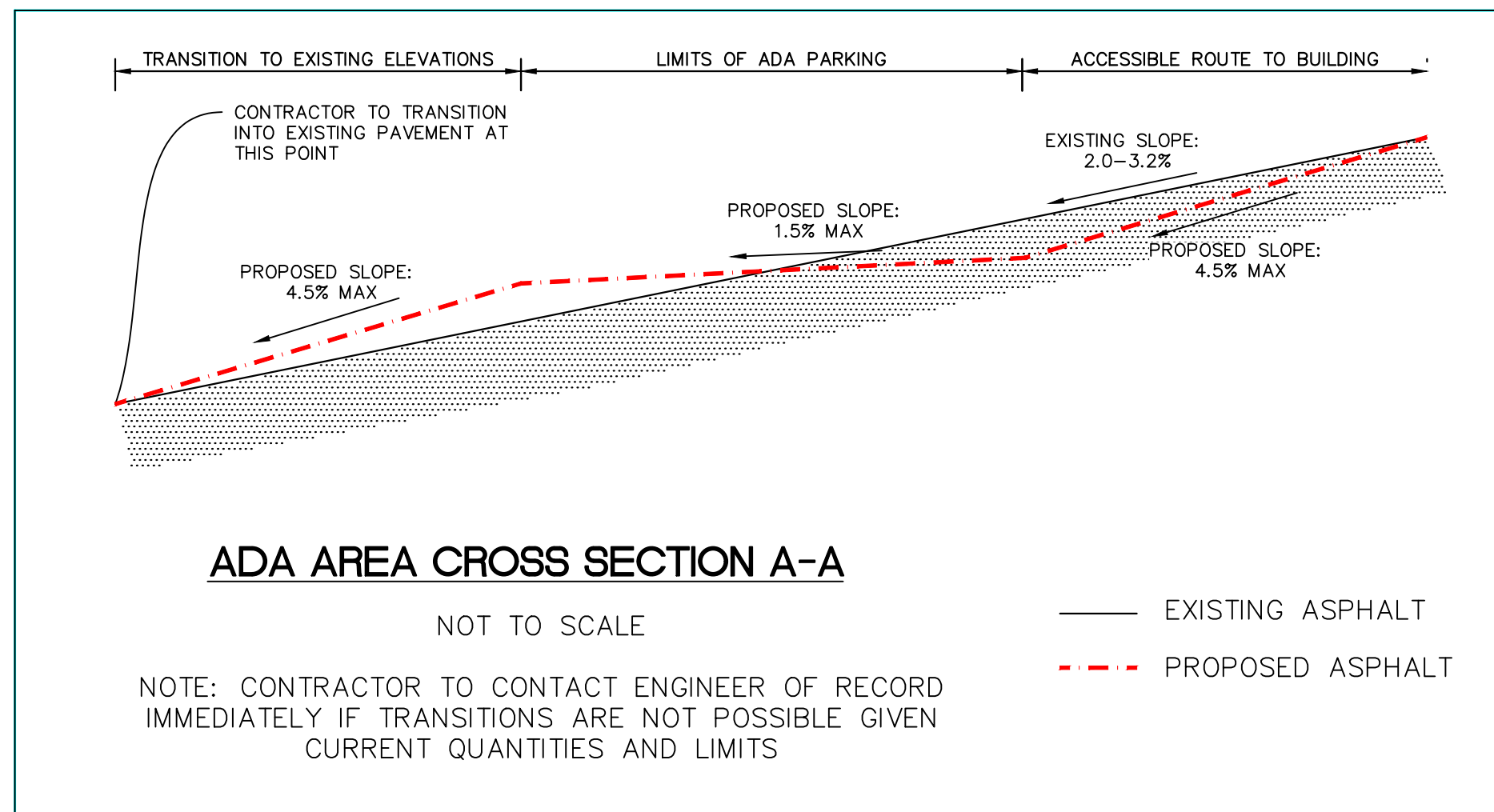
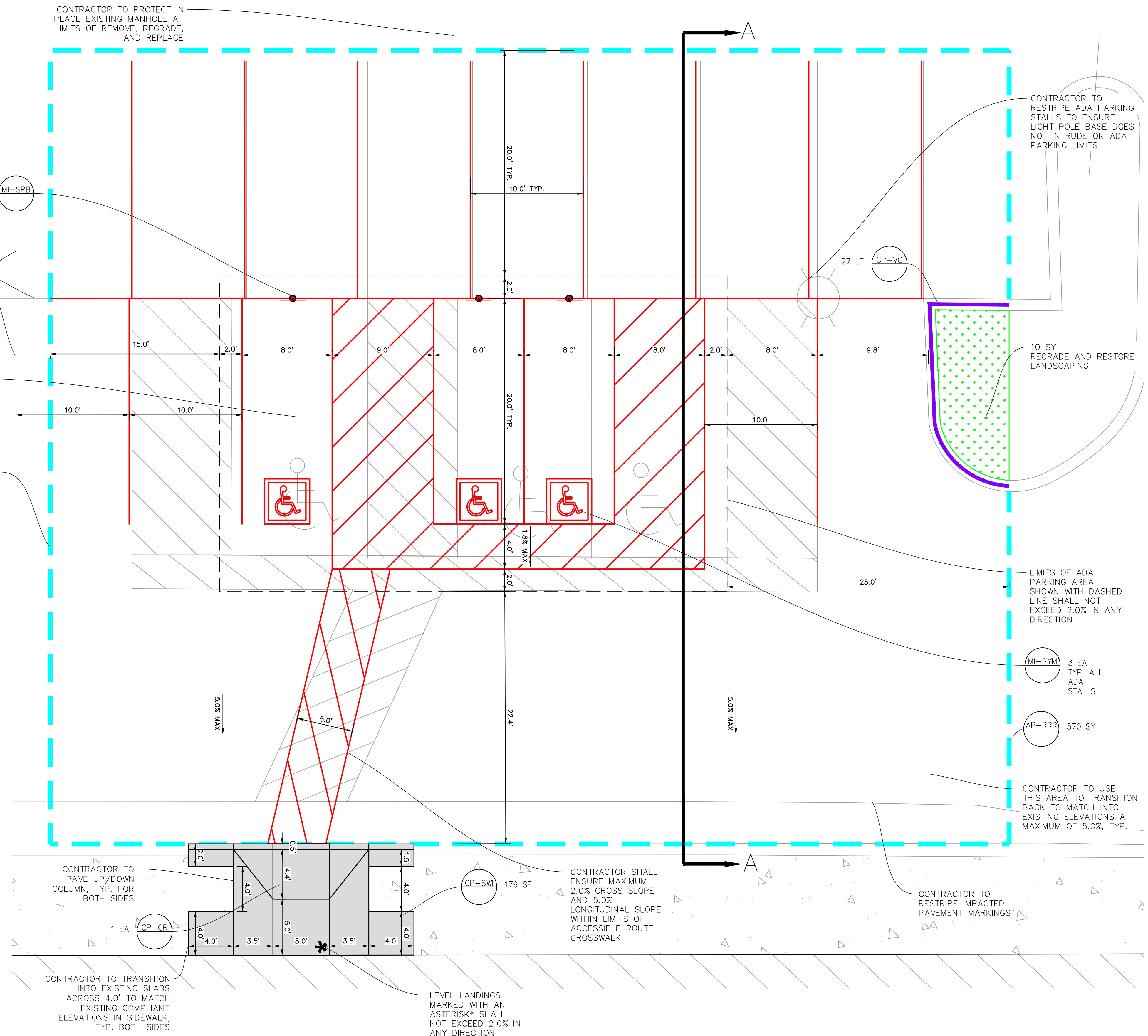
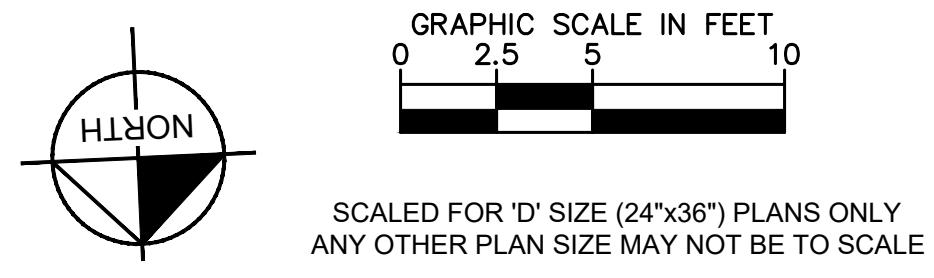
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- PROPOSED PAVING IMPROVEMENTS**
- ASPHALT IMPROVEMENTS**
- CRACK SEAL
 - 2.0" MILL AND OVERLAY
 - CUT AND PATCH
 - REMOVE, REGRADE, AND REPLACE
 - REMOVE, REGRADE, AND REPLACE LIMITS
 - ALTERNATE A: 2.5" REMOVE AND REPLACE
 - ALTERNATE B: 2.5" REMOVE AND REPLACE
 - ALTERNATE C: 3.5" REMOVE AND REPLACE
 - ALTERNATE D: CRACK SEAL
- CONCRETE IMPROVEMENTS**
- CONCRETE COLLAR
 - REMOVE, REGRADE, AND REPLACE SIDEWALK
 - LEVEL LANDING (MAX. 2.0% SLOPE IN ALL DIRECTIONS)
 - REMOVE AND REPLACE VERTICAL CURB
- MISCELLANEOUS IMPROVEMENTS**
- ADA SIGN AND POST WITH BOLLARD
 - REVISED STRIPING
 - ADA SYMBOL WITH BACKGROUND
 - REGRADE AND RESTORE LANDSCAPING

CONCRETE DESIGN INTENT: MATCH EXISTING ELEVATIONS AT THE FACE OF BUILDING AND REGRADE SIDEWALK SOUTH TO PROPOSED LIMITS AT LESS THAN 2.0% - 1.5% TARGET - GRADES TO BE MADE UP BY RAISING TOP AND BOTTOM CURB ELEVATIONS TO ENSURE CURB HEIGHT DOES NOT EXCEED 6.0".

ASPHALT DESIGN INTENT: MATCH PROPOSED ELEVATIONS AT NORTH CURB FACE AND LOWER/RAISE ASPHALT TOWARDS THE SOUTH IN ORDER TO REGRADE ACCESSIBLE ROUTE AND ADA PARKING AREA AS SHOWN IN CROSS SECTION A-A.

ADA AREA 2 (ALTERNATE 2A)



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ADA IMPROVEMENTS									
THE CENTER AT HOBBS BROOK STURBRIDGE, MA									
SHEET NUMBER C1-03									

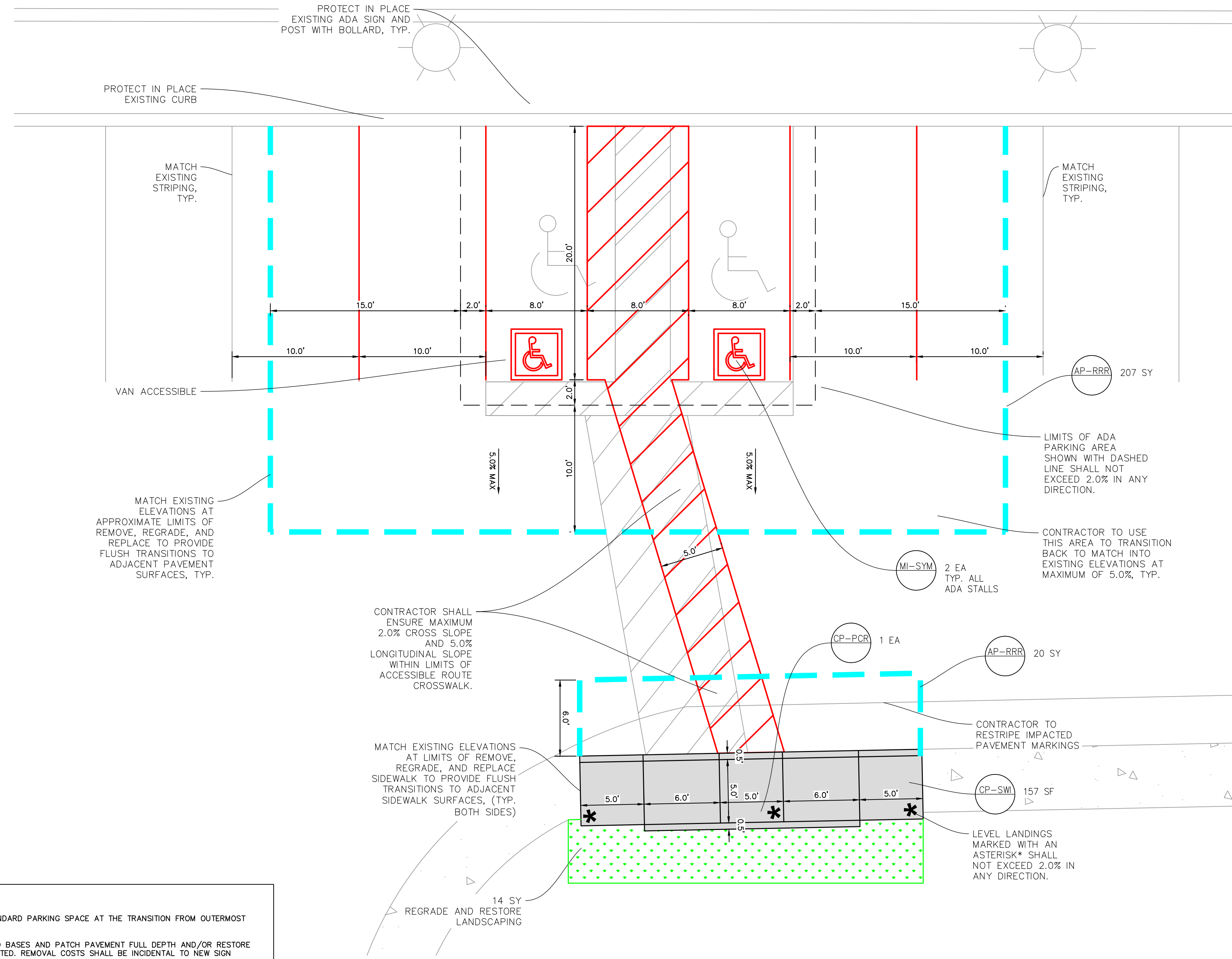
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PROPOSED PAVING IMPROVEMENTS

	DETAIL REFERENCE (SEE APPROPRIATE DETAIL SHEET)
ASPHALT IMPROVEMENTS	
	CRACK SEAL
	2.0" MILL AND OVERLAY
	CUT AND PATCH
	REMOVE, REGRADE, AND REPLACE
	REMOVE, REGRADE, AND REPLACE LIMITS
	ALTERNATE A: 2.5" REMOVE AND REPLACE
	ALTERNATE B: 2.5" REMOVE AND REPLACE
	ALTERNATE C: 3.5" REMOVE AND REPLACE
	ALTERNATE D: CRACK SEAL
CONCRETE IMPROVEMENTS	
	CONCRETE COLLAR
	REMOVE, REGRADE, AND REPLACE SIDEWALK
	LEVEL LANDING (MAX. 2.0% SLOPE IN ALL DIRECTIONS)
	REMOVE AND REPLACE VERTICAL CURB
MISCELLANEOUS IMPROVEMENTS	
	ADA SIGN AND POST WITH BOLLARD
	REVISED STRIPING
	ADA SYMBOL WITH BACKGROUND
	REGRADE AND RESTORE LANDSCAPING

**ADA AREA 3
(ALTERNATE 2B)**

GRAPHIC SCALE IN FEET
0 2.5 5 10
SCALED FOR 'D' SIZE (24"x36") PLANS ONLY
ANY OTHER PLAN SIZE MAY NOT BE TO SCALE



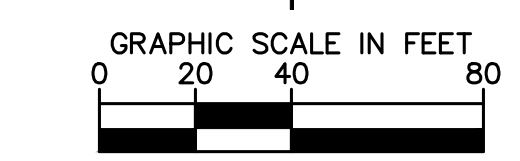
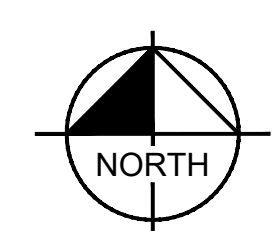
- ADA STRIPING AND REPAIR NOTES**
- CONTRACTOR SHALL MAINTAIN A MINIMUM 9' WIDE STANDARD PARKING SPACE AT THE TRANSITION FROM OUTERMOST ADA PARKING SPACE IN EVERY AISLE.
 - CONTRACTOR SHALL REMOVE EXISTING SIGN POSTS AND BASES AND PATCH PAVEMENT FULL DEPTH AND/OR RESTORE LANDSCAPING WHERE REVISED SIGN LOCATIONS ARE NOTED. REMOVAL COSTS SHALL BE INCIDENTAL TO NEW SIGN INSTALLATIONS.
 - DIMENSIONS NOTED ARE TO THE CENTERLINE OF MARKINGS.
 - MAXIMUM 2.0% SLOPE IN ALL DIRECTIONS WITHIN ALL ADA PARKING AREAS.
 - SIDEWALKS SHALL NOT EXCEED 2.0% CROSS SLOPE AND 5.0% LONGITUDINAL SLOPE, UNLESS OTHERWISE SHOWN. SIDEWALK LEVEL LANDINGS MARKED WITH AN ASTERISK* SHALL NOT EXCEED 2.0% IN ANY DIRECTION.
 - ANY ADDITIONAL SIDEWALK AREAS DISTURBED OR DAMAGED DURING CONSTRUCTION MUST BE REPLACED TO EXISTING CONDITIONS AT NO ADDITIONAL COST TO THE OWNER.
 - CONTRACTOR TO USE APPROPRIATE PAINT COLORS TO MATCH THE EXISTING FEATURES. ADA BARRIER FREE AREAS TO COMPLY WITH ALL LOCAL AND FEDERAL ADA STANDARDS. PAVEMENT MARKINGS SHALL BE A HIGH QUALITY PAINT RECOMMENDED BY THE PAINT MANUFACTURER. PAINT COLORS SHALL CLOSELY MATCH THE FEDERAL STANDARD 595B COLOR NUMBER AS FOLLOWS: BLUE-COLOR 15180, WHITE-COLOR 37925 AND YELLOW-COLOR 33538.
 - IN AREAS NOT BEING RESURFACED BUT REQUIRING RESTRIPING, CONTRACTOR SHALL REMOVE EXISTING STRIPING BY EITHER SAND BLASTING, WATER BLASTING, GRINDING OR BY ANOTHER APPROVED METHOD OF STRIPING REMOVAL. WHERE THE STRIPING REMOVAL CHANGES THE SURFACE TEXTURE OF THE EXISTING ASPHALT, CONTRACTOR SHALL SEAL COAT THE AREA PRIOR TO RESTRIPING. CONTRACTOR TO SUBMIT STRIPING REMOVAL METHOD TO PROPERTY MANAGER AND ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION.
 - CONTRACTOR SHALL MATCH EXISTING ADA STRIPING IN ALL PAVEMENT REPAIR AREAS UNLESS REVISED STRIPING IS SPECIFICALLY SHOWN ON THE PLANS.

THIS PLAN SET IS BASED ON AUTOCAD FILES OBTAINED BY KIMLEY-HORN AND ASSOCIATES, INC. FROM CIM GROUP

DRAWING RECEIVED ON: JULY 27, 2016
THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE EXISTING INFORMATION SHOWN ON THESE PLANS IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR WILL BE RESPONSIBLE FOR FIELD VERIFYING EXISTING SITE CONDITIONS BEFORE COMMENCING CONSTRUCTION. THE CONTRACTOR MUST ALSO CALL 811 AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES.

KHA PROJECT 168719048	DATE 04/04/2020	SCALE AS SHOWN	DESIGNED BY ATO	DRAWN BY ATO	CHECKED BY JHP	EROSION CONTROL	07/07/20 WAD
							NO.
<p>Kimley»Horn</p> <p>© 2020 KIMLEY-HORN AND ASSOCIATES, INC. 1001 WARRENVILLE ROAD, SUITE 350, LISLE, IL 60532 PHONE: 630-487-5550 WWW.KIMLEY-HORN.COM</p>						<p>ADA IMPROVEMENTS</p>	
SHEET NUMBER						C1-04	



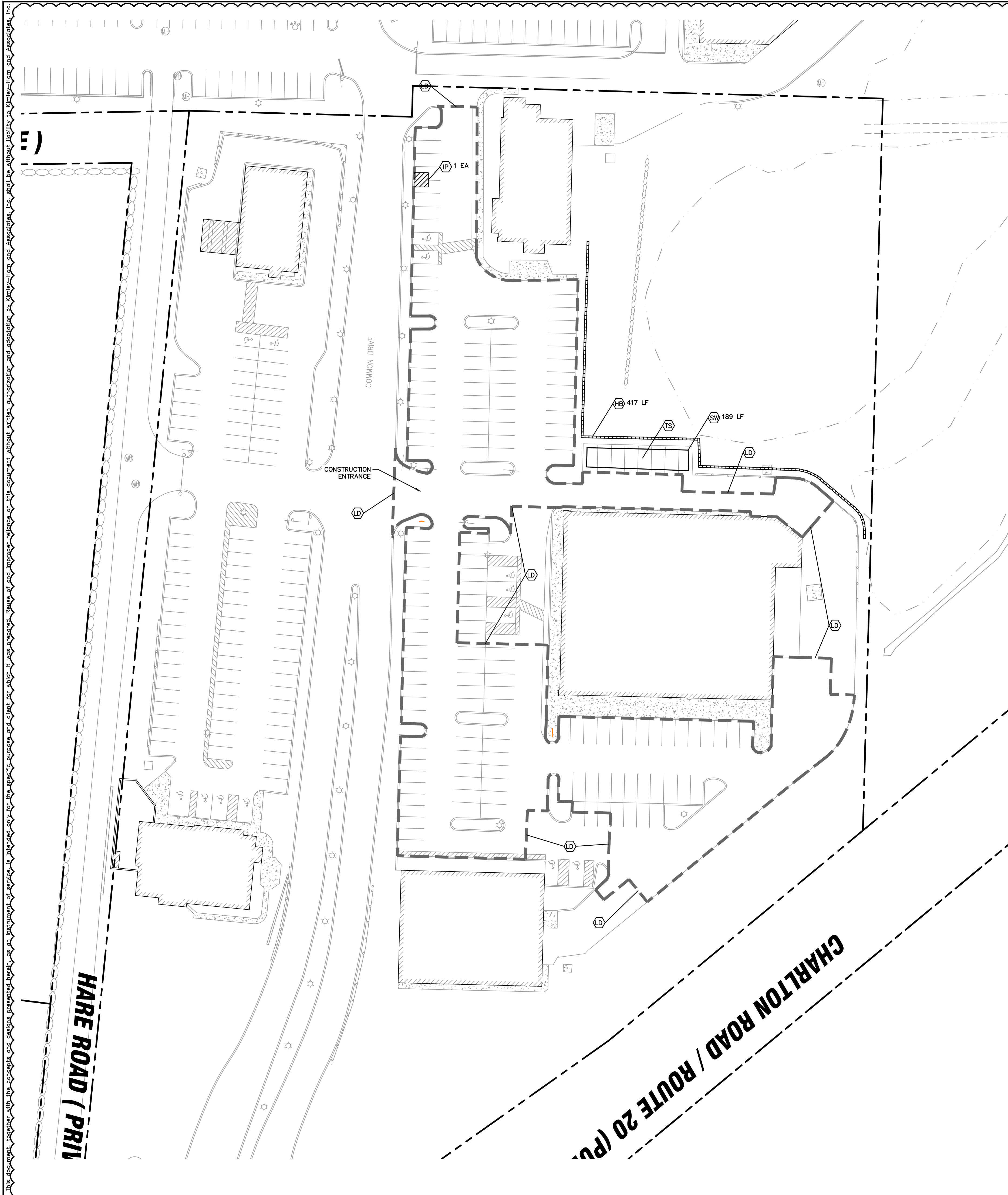


EROSION CONTROL LEGEND	
	LD LIMITS OF DISTURBANCE
	HB HALE BALE (SEE EROSION CONTROL DETAILS)
	SW 9" STRAW SWALE (SEE EROSION CONTROL DETAILS)
	IP INLET PROTECTION (SEE EROSION CONTROL DETAILS)
	SS TEMPORARY SOIL STOCKPILE
	CE CONSTRUCTION ENTRANCE (SEE EROSION CONTROL DETAILS)

EROSION CONTROL NOTES	
1.	CONSTRUCTION ENTRANCE SHALL BE LOCATED SO AS TO PROVIDE THE LEAST AMOUNT OF DISTURBANCE TO THE FLOW OF TRAFFIC IN AND OUT OF THE SITE. ADDITIONALLY, CONSTRUCTION ENTRANCE SHALL BE LOCATED TO COINCIDE WITH THE PHASING OF THE PAVEMENT REPLACEMENT.
2.	POST CONSTRUCTION STORM WATER POLLUTION CONTROL MEASURES INCLUDE STABILIZATION BY PERMANENT PAVING, DRAINAGE SYSTEM STRUCTURE, OR LANDSCAPING.
3.	TEMPORARY AND PERMANENT STABILIZATION PRACTICES AND BMP'S SHALL BE INSTALLED AT THE EARLIEST POSSIBLE TIME DURING THE CONSTRUCTION SEQUENCE. AS AN EXAMPLE, PERIMETER SILT FENCE SHALL BE INSTALLED BEFORE COMMENCEMENT OF ANY GRADING ACTIVITIES. OTHER BMP'S SHALL BE INSTALLED AS SOON AS PRACTICABLE AND SHALL BE MAINTAINED UNTIL FINAL SITE STABILIZATION IS ATTAINED. CONTRACTOR SHALL ALSO REFERENCE CIVIL AND LANDSCAPE PLANS SINCE PERMANENT STABILIZATION IS PROVIDED BY LANDSCAPING, THE BUILDING(S), AND SITE PAVING.
4.	BMP'S HAVE BEEN LOCATED AS INDICATED ON THIS PLAN IN ACCORDANCE WITH GENERALLY ACCEPTED ENGINEERING PRACTICES IN ORDER TO MINIMIZE SEDIMENT TRANSFER. FOR EXAMPLE: SILT FENCES LOCATED AT TOE OF SLOPE AND INLET PROTECTION FOR INLETS RECEIVING SEDIMENT FROM SITE RUN-OFF.
5.	THE PLACEMENT OF EROSION/SEDIMENTATION CONTROLS SHALL BE IN ACCORDANCE WITH THE APPROVED EROSION AND SEDIMENTATION CONTROL PLAN.
6.	ANY MAJOR VARIATION IN MATERIALS OR LOCATIONS OF CONTROLS OR FENCES FROM THOSE SHOWN ON THE APPROVED PLANS WILL REQUIRE A REVISION AND MUST BE APPROVED BY THE REVIEWING ENGINEER, ENVIRONMENTAL SPECIALIST, OR ARBORIST AS APPROPRIATE. MAJOR REVISIONS MUST BE APPROVED BY THE PLANNING AND DEVELOPMENT DEPARTMENT AND THE DRAINAGE UTILITY DEPARTMENT. MINOR CHANGES OR ADDITIONAL CONTROL MEASURES TO BE MADE AS FIELD REVISIONS TO THE EROSION AND SEDIMENTATION CONTROL PLAN MAY BE REQUIRED BY THE ENVIRONMENTAL INSPECTOR DURING THE COURSE OF CONSTRUCTION TO CORRECT CONTROL INADEQUACIES AT NO ADDITIONAL COST TO THE OWNER.
7.	CONTRACTOR SHALL PLACE EROSION CONTROL BLANKET (NORTH AMERICAN GREEN S1508B OR EQUAL) ON ALL SITE AREAS WITH SLOPES GREATER THAN 4:1, AND IN THE BOTTOM AND SIDE SLOPES OF ALL SWALES.
8.	PRIOR TO FINAL ACCEPTANCE, HAUL ROADS AND WATERWAY CROSSINGS CONSTRUCTED FOR TEMPORARY CONTRACTOR ACCESS MUST BE REMOVED, ACCUMULATED SEDIMENT REMOVED FROM THE WATERWAY AND THE AREA RESTORED TO THE ORIGINAL GRADE AND REVEGETATED. ALL LAND CLEARING SHALL BE DISPOSED OF IN APPROVED SPOIL DISPOSAL SITES.
9.	PERMANENT, FINAL PLANT COVERING OR STRUCTURES SHALL BE INSTALLED PRIOR TO FINAL ACCEPTANCE.
10.	ALL CONTROL DEVICES THAT FUNCTION SIMILARLY TO SILT FENCE OR FIBER ROLLS MUST BE REPAIRED, REPLACED OR SUPPLEMENTED WITH EFFECTIVE CONTROLS WHEN THEY BECOME NONFUNCTIONAL OR THE SEDIMENT REACHES ONE-THIRD THE HEIGHT OF THE DEVICE. THESE REPAIRS MUST BE MADE WITHIN 24 HOURS OF THE RAINFALL EVENT OR AS SOON AS FIELD CONDITIONS ALLOW ACCESS.
11.	ALL SEDIMENT DELTAS AND DEPOSITS MUST BE REMOVED FROM SURFACE WATERS, DRAINAGE WAYS, CATCH BASINS AND OTHER DRAINAGE SYSTEMS. ALL AREAS WHERE SEDIMENT REMOVAL RESULTED IN EXPOSED SOIL MUST BE REESTABLISHED. THE REMOVAL AND STABILIZATION MUST TAKE PLACE IMMEDIATELY, BUT NO MORE THAN 7 DAYS AFTER THE RAINFALL EVENT UNLESS PRECLUDED BY LEGAL, REGULATORY OR PHYSICAL ACCESS CONSTRAINTS. ALL REASONABLE EFFORTS MUST BE USED TO OBTAIN ACCESS. ONCE ACCESS IS OBTAINED, REMOVAL AND STABILIZATION MUST TAKE PLACE IMMEDIATELY, BUT NO MORE THAN 7 DAYS LATER. CONTRACTOR IS RESPONSIBLE FOR CONTACTING ALL APPROPRIATE AUTHORITIES AND RECEIVING THE APPLICABLE PERMITS PRIOR TO CONDUCTING ANY WORK.
12.	ACCUMULATIONS OF TRACKED AND DEPOSITED SEDIMENT MUST BE REMOVED FROM OFF-SITE PAVED SURFACES WITHIN 24 HOURS OR SOONER IF REQUIRED. SEDIMENT TRACKING MUST BE MINIMIZED BY THE APPROPRIATE MANAGEMENT PRACTICE, LIKE A DEDICATED SITE EXIT WITH AN AGGREGATE SURFACE OR DESIGNATED OFFSITE PARKING AREA. CONTRACTOR IS RESPONSIBLE FOR STREET SWEEPING AND/OR SCRAPING IF YOUR PRACTICES ARE NOT ADEQUATE TO PREVENT SEDIMENT FROM BEING TRACKED FROM THE SITE.
13.	SURFACE WATERS, DRAINAGE DITCHES AND CONVEYANCE SYSTEMS MUST BE INSPECTED FOR SEDIMENT DEPOSITS.
14.	THE CONTRACTOR SHALL INSTALL AND MAINTAIN ALL EROSION CONTROL MEASURES AS INDICATED ON THIS SHEET IN ACCORDANCE WITH THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP) PREPARED BY KIMLEY-HORN AND ASSOCIATES, INC. THE CONTRACTOR IS RESPONSIBLE FOR IMPLEMENTING THE PROVISIONS INDICATED IN THE SWPPP, INCLUDING EROSION CONTROL MEASURES AND INSPECTION FREQUENCY, AS REQUIRED BY THE IEPA NPDES PHASE II PERMIT PROGRAM REQUIREMENTS.
15.	PUMPING SEDIMENT LADEN WATER INTO ANY STORMWATER FACILITY THAT IS NOT DESIGNATED TO BE A SEDIMENT TRAP, DRAINAGEWAY, OR OFFSITE AREA EITHER DIRECTLY OR INDIRECTLY WITHOUT FILTRATION IS PROHIBITED.
16.	SOIL STOCKPILES SHALL NOT BE LOCATED IN A DRAINAGEWAY, FLOOD PLAIN AREA OR A DESIGNATED BUFFER, UNLESS OTHERWISE APPROVED, UNDER SPECIFIC CONDITIONS TO BE ESTABLISHED BY THE DIRECTOR OR ADMINISTRATOR.
17.	STOCKPILES TO REMAIN IN PLACE FOR MORE THAN THREE DAYS SHALL BE PROVIDED WITH SESC MEASURES. MATERIAL IS TO BE HAULED OFF IMMEDIATELY AND LEGALLY IF NO STOCKPILE IS TO REMAIN IN PLACE.
18.	ALL TEMPORARY SESC MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL STABILIZATION IS ACHIEVED. TRAPPED SEDIMENT AND OTHER DISTURBED SOILS RESULTING FROM TEMPORARY MEASURES SHALL BE PROPERLY DISPOSED OF PRIOR TO PERMANENT STABILIZATION.
19.	WATER REMOVED FROM TRAPS, BASINS, AND OTHER WATER HOLDING DEPRESSIONS OR EXCAVATIONS MUST FIRST PASS THROUGH A SEDIMENT CONTROL AND/OR FILTRATION DEVICE. WHEN DEWATERING DEVICES ARE USED, DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION.
20.	SITE STABILIZATION REQUIREMENTS ARE AS FOLLOWS:
20.1.	WHERE THE INITIATION OF STABILIZATION MEASURE BY THE 7TH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY OR PERMANENTLY CEASES ON A PORTION OF THE SITE IS PRECLUDED BY SNOW COVER, STABILIZATION MEASURE SHALL BE INITIATED AS SOON AS PRACTICABLE.
20.2.	WHERE CONSTRUCTION ACTIVITY WILL RESUME ON A PORTION OF THE SITE WITHIN 14 DAYS FROM WHEN ACTIVITIES CEASED, (E.G. THE TOTAL TIME PERIOD THAT CONSTRUCTION ACTIVITY IS TEMPORARILY CEASED IS LESS THAN 14 DAYS) THEN STABILIZATION MEASURES DO NOT HAVE TO BE INITIATED ON THAT PORTION OF THE SITE BY THE 7TH DAY AFTER CONSTRUCTION ACTIVITY TEMPORARILY CEASED.

EROSION CONTROL SCHEDULE AND SEQUENCING:

I. ROUGH GRADING	CONSTRUCTION ENTRANCE/EXIT, SILT FENCE PROTECTION, CONCRETE WASHOUT AREA AND TREE PROTECTION SHALL BE INSTALLED PRIOR TO THE INITIATION OF ROUGH GRADING, AS NEEDED. TEMPORARY EROSION CONTROL MEASURES TO BE INSTALLED UPON COMPLETION OF ROUGH GRADING AND AS NECESSARY THROUGHOUT CONSTRUCTION.
II. UTILITY INSTALLATION	ALL PRIOR EROSION CONTROL MEASURES INSTALLED ABOVE TO BE MAINTAINED AS NECESSARY DURING UTILITY INSTALLATION. STORM STRUCTURE INLET PROTECTION SHALL BE INSTALLED AS STORM DRAINAGE SYSTEM IS CONSTRUCTED.
III. PAVING	ALL PRIOR EROSION CONTROL MEASURES INSTALLED ABOVE TO BE MAINTAINED AS NECESSARY DURING PAVING AND THROUGHOUT THE REMAINDER OF THE PROJECT.
IV. FINAL GRADING/SOIL STABILIZATION/LANDSCAPING	ALL TEMPORARY EROSION CONTROL MEASURES TO BE REMOVED AT THE CONCLUSION OF THE PROJECT AS DIRECTED BY THE LOCAL MUNICIPALITY.



07/07/2014

EROSION CONTROL

NO. _____

REVISIONS _____

DATE _____

BY _____

Kimley»Horn

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KHA PROJECT	168719048
DATE	04/04/2020
SCALE	AS SHOWN
DESIGNED BY	ATO
DRAWN BY	ATO
CHECKED BY	JHP

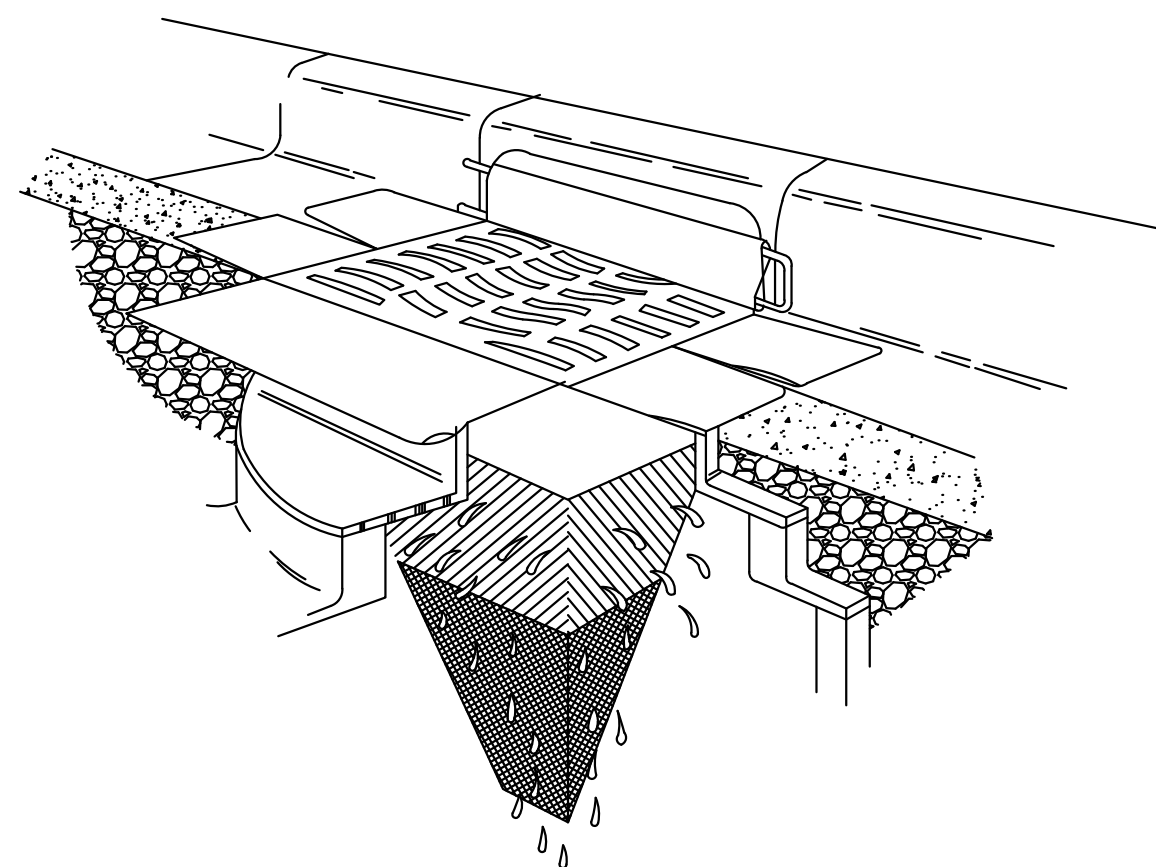
EROSION CONTROL PLAN

**THE CENTER AT
HOBBS BROOK
STURBRIDGE, MA**

SHEET NUMBER
C1-05

SILTSACK

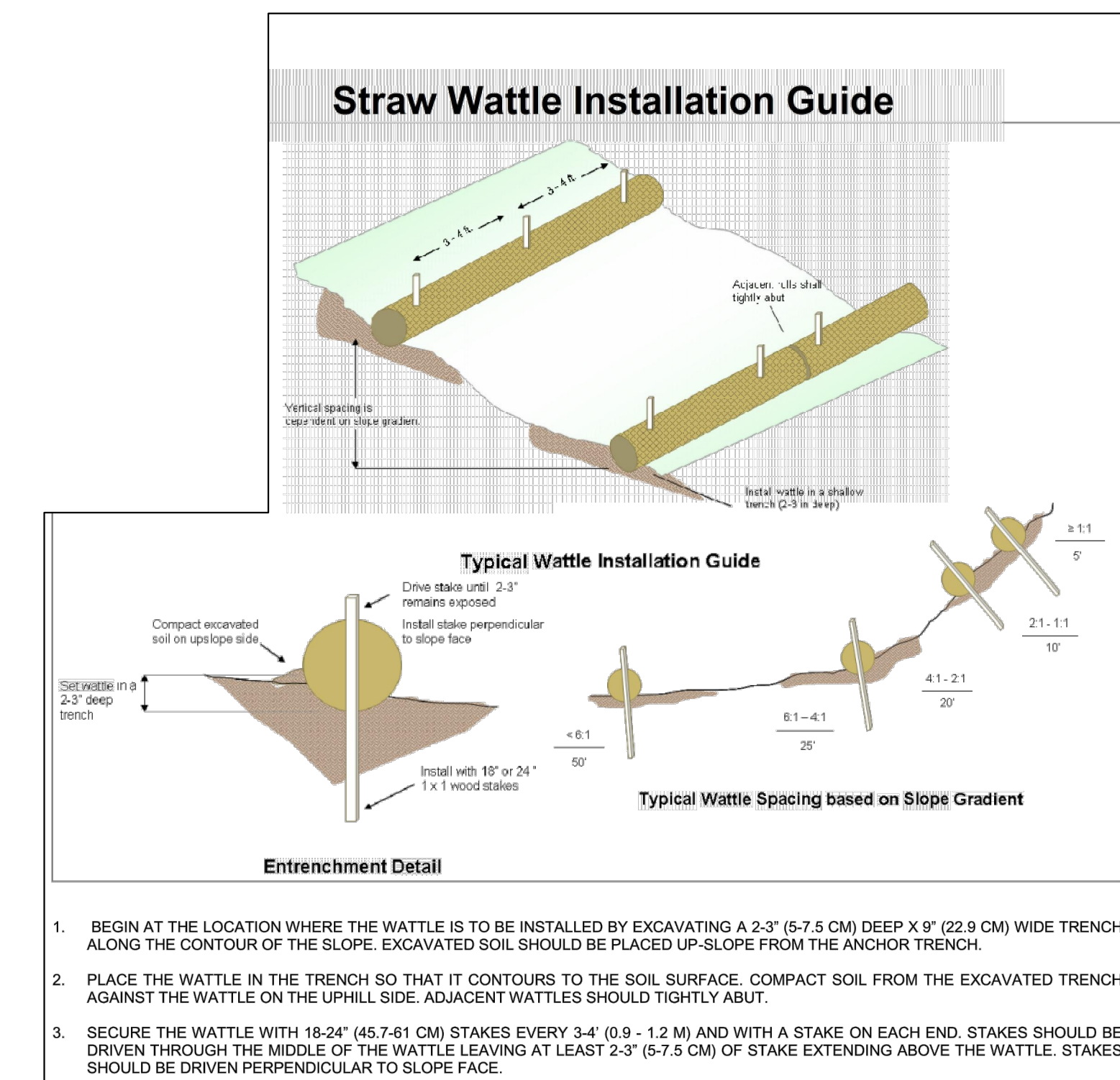
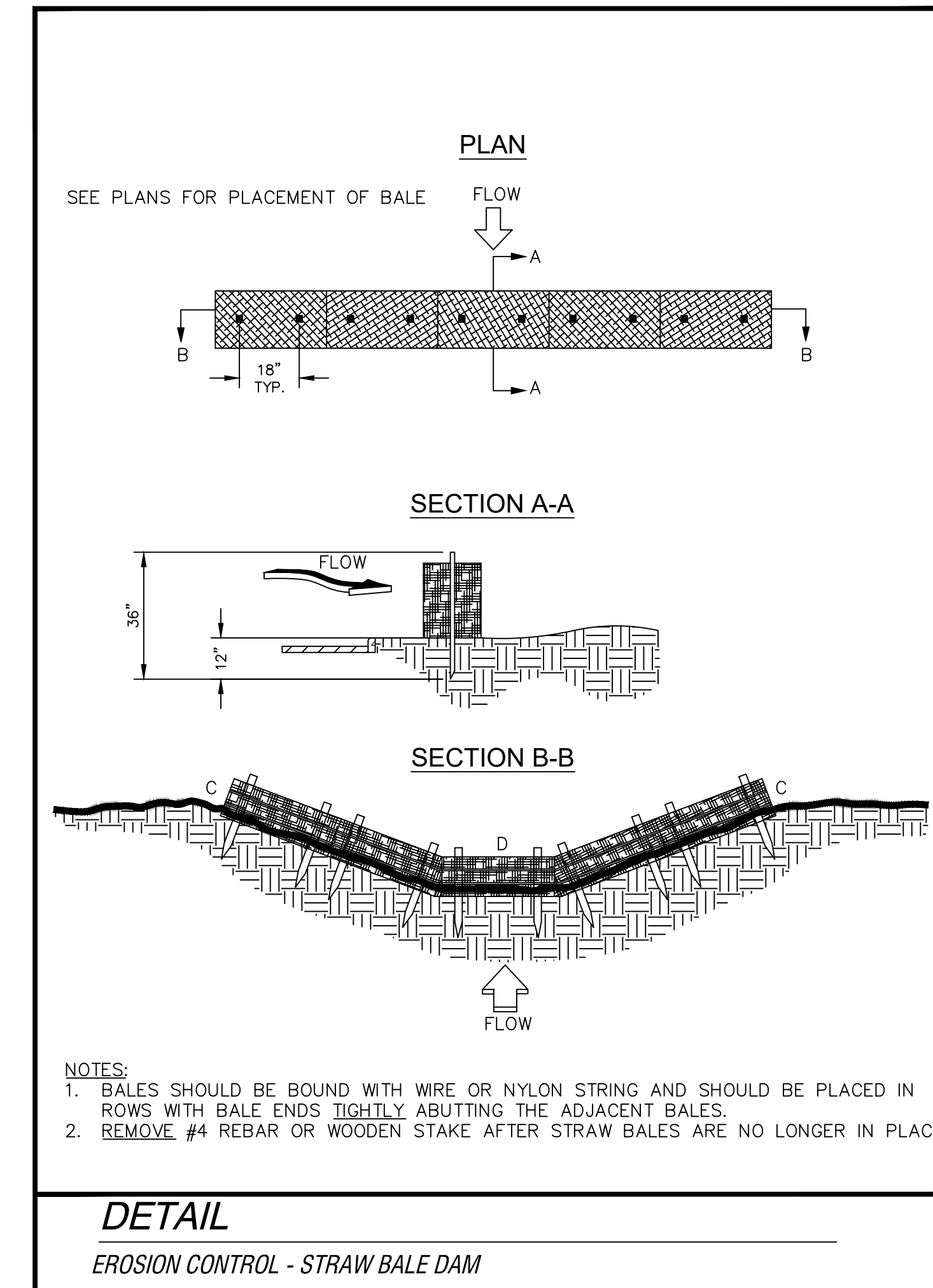
PROTECT CATCH BASINS FROM SEDIMENT & TRASH



- EASY TO INSTALL - EASY TO MAINTAIN
- ECONOMICAL
- FABRICATED TO FIT ANY SIZE OR SHAPE
- PERMEABILITY OF 200 GPM/SF [HI-FLOW STYLE]
- REPLACES ALL ROCK OR GEOTEXTILES
- REUSABLE

DETAIL

SILT SACK (OR APPROVED EQUAL)



DETAIL

EROSION CONTROL - 9" DIAMETER WATTLE

07/07/2014	EROSION CONTROL	NO.	REVISIONS	BY	DATE
1					

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KHA PROJECT	DATE	SCALE	DESIGNED BY	DRAWN BY	CHECKED BY
168719048	04/04/2020	AS SHOWN	ATO	ATO	JHP

EROSION CONTROL
DETAILS

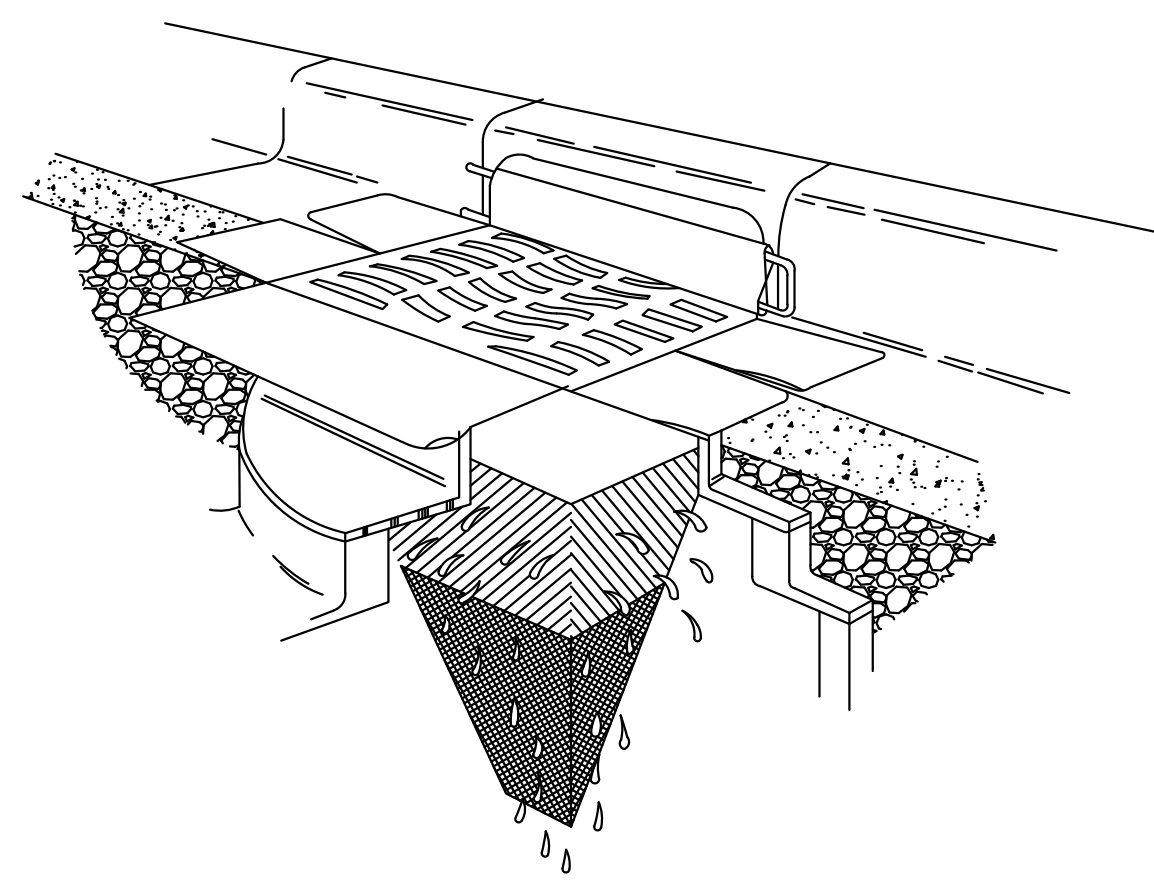
THE CENTER AT
HOBBS BROOK
STURBRIDGE, MA

SHEET NUMBER
C2-00

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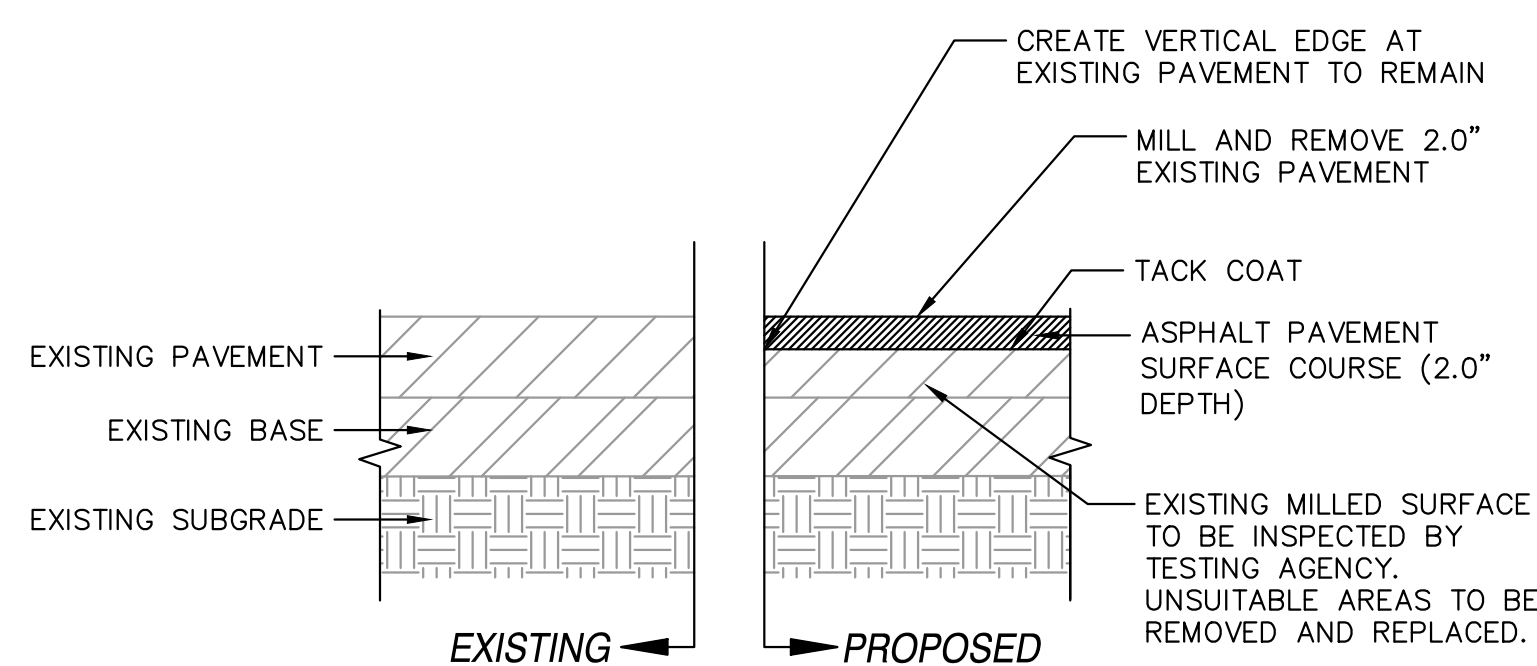
PROTECT CATCH BASINS
FROM SEDIMENT & TRASH



- EASY TO INSTALL – EASY TO MAINTAIN
- ECONOMICAL
- FABRICATED TO FIT ANY SIZE OR SHAPE
- PERMEABILITY OF 200 GPM/SF [HI-FLOW STYLE]
- REPLACES ALL ROCK OR GEOTEXTILES
- REUSABLE

DETAIL

SILT SACK (OR APPROVED EQUAL)

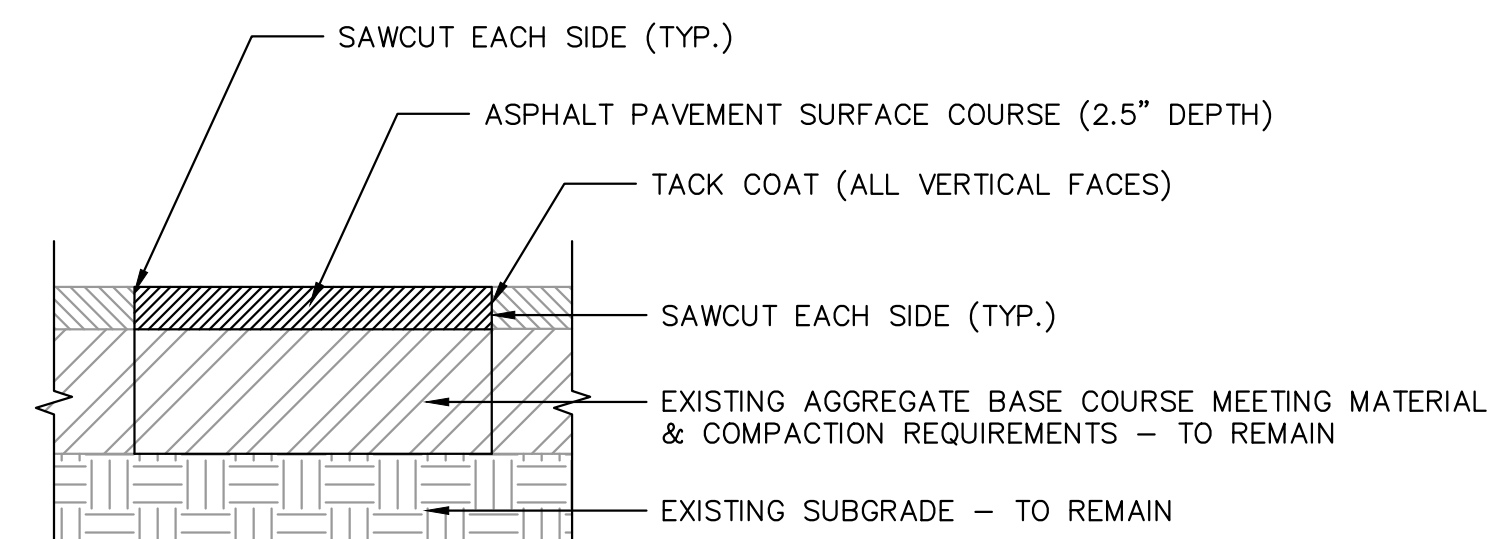


- NOTES:
1. SURFACE COURSE TO BE INSTALLED AT SAME TIME AS ADJACENT ASPHALT REPAIRS SURFACE COURSE.

DETAIL

ASPHALT PAVEMENT - 2.0" MILL AND OVERLAY

AP-MO

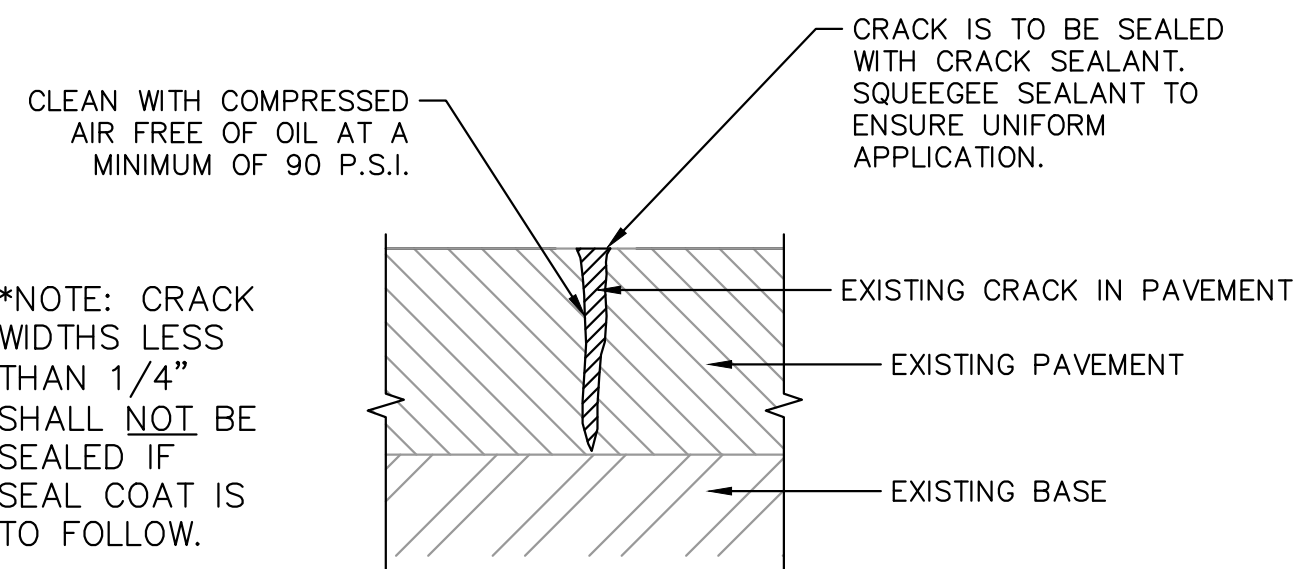


- NOTE:
1. CUT AND PATCH AREAS SHOWN ON THE PLANS ARE APPROXIMATE IN SIZE AND LOCATION AND MAY BE ADJUSTED AT THE TIME OF CONSTRUCTION.
 2. SURFACE COURSE SHALL BE INSTALLED AT THE SAME TIME AS ADJACENT PAVEMENT REPAIR SURFACE COURSES.
 3. IF AGGREGATE BASE COURSE IS NOT PRESENT CONTRACTOR TO NOTIFY ENGINEER OF RECORD IMMEDIATELY.
 4. THE AGGREGATE BASE COURSE SHALL BE INSPECTED BY THE TESTING AGENCY, OWNER, OR DESIGN CONSULTANT AFTER ASPHALT REMOVAL AND BASE PREPARATION, PRIOR TO ASPHALT REPLACEMENT.

DETAIL

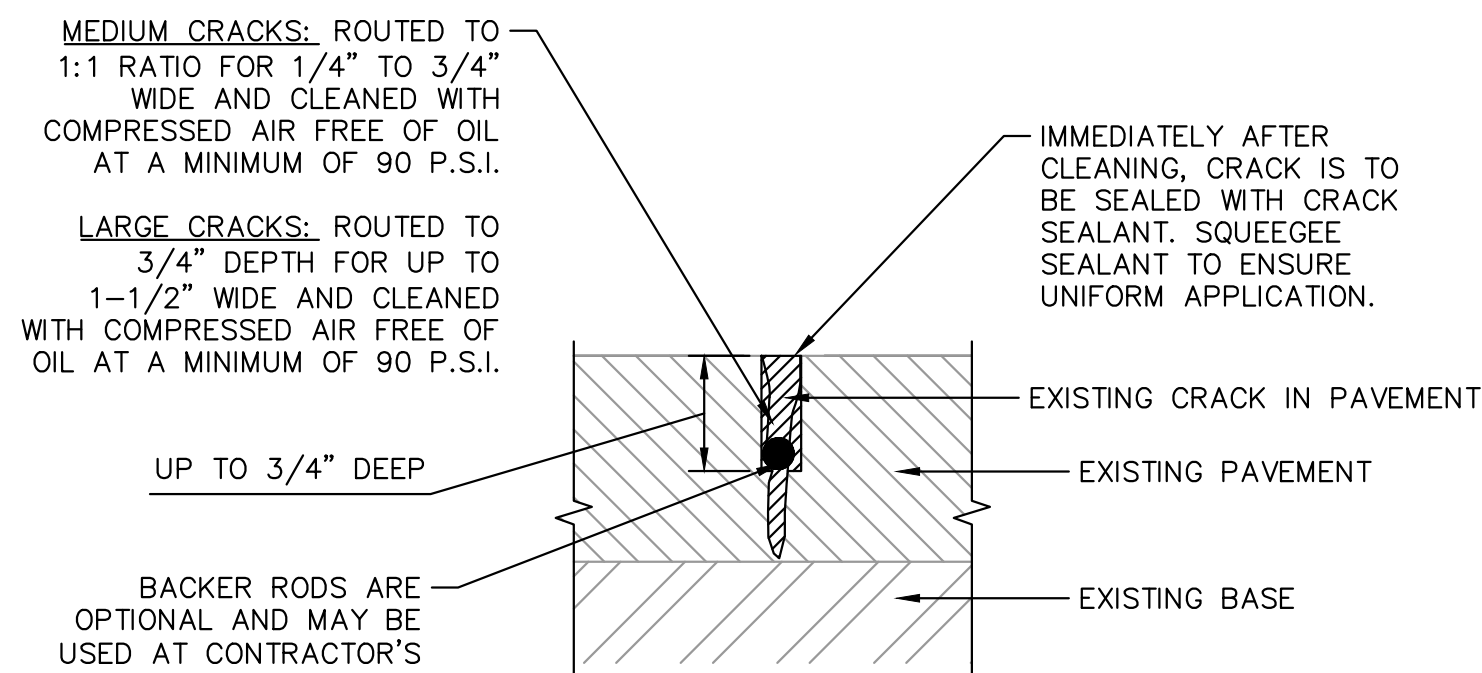
ASPHALT PAVEMENT - CUT AND PATCH

AP-CP



*NOTE: CRACK WIDTHS LESS THAN 1/4" SHALL NOT BE SEALED IF SEAL COAT IS TO FOLLOW.

SMALL CRACKS
(LESS THAN 1/4")

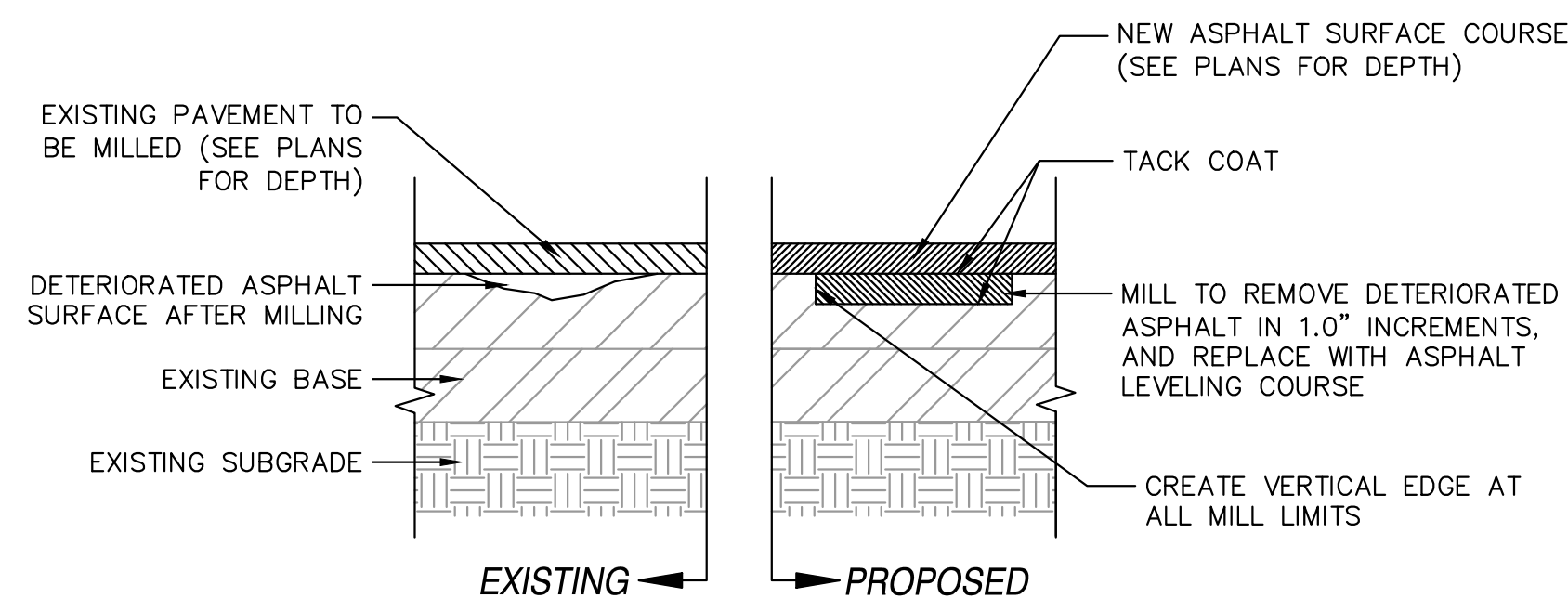


MEDIUM TO LARGE CRACKS
(1/4" TO 1-1/2")

DETAIL

ASPHALT PAVEMENT - CRACK SEAL

AP-CS

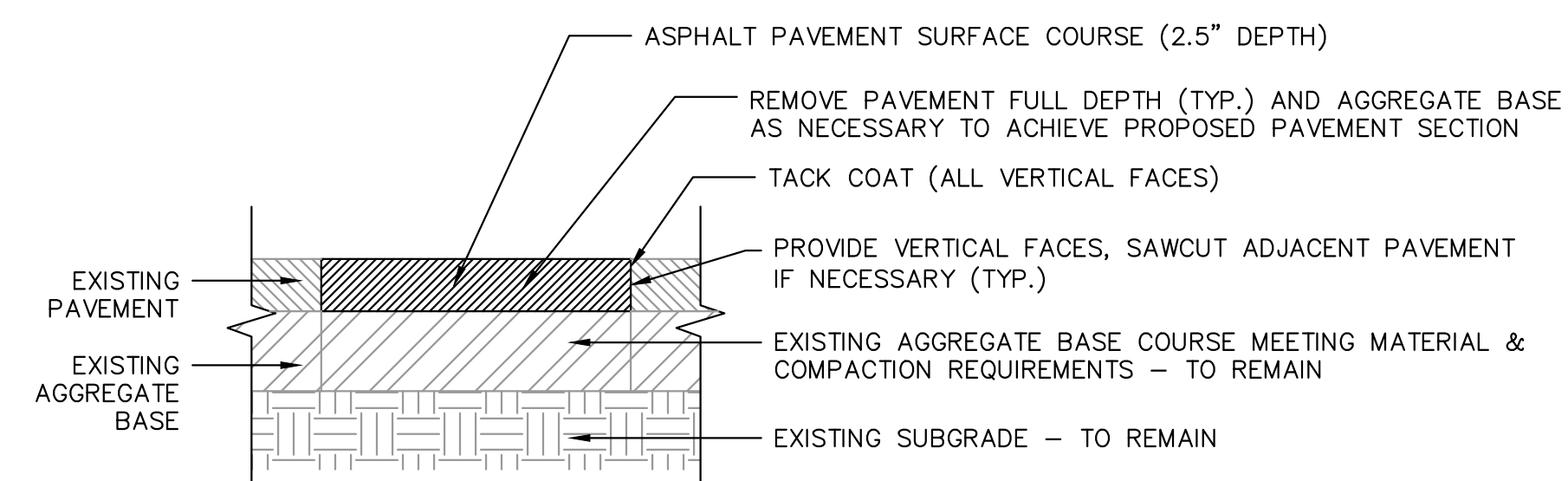


- NOTES:
1. ALL INCREMENTAL MILLING AND REPLACEMENT AREAS SHALL BE DETERMINED AND VERIFIED WITH THE OWNER OR DESIGN CONSULTANT PRIOR TO COMPLETION. ITEM SHALL BE PAID ACCORDING TO ESTABLISHED UNIT PRICE (\$/S.Y./INCH)
 2. AFTER AREAS HAVE BEEN PREPARED TO THE SATISFACTION OF THE OWNER OR DESIGN CONSULTANT APPLY TACK COAT AT THE RATE OF 0.20 GAL/S.Y. PRIOR TO ASPHALT REPLACEMENT.

DETAIL

ASPHALT PAVEMENT - 1.0" INCREMENTAL MILL AND REPLACE

AP-IM

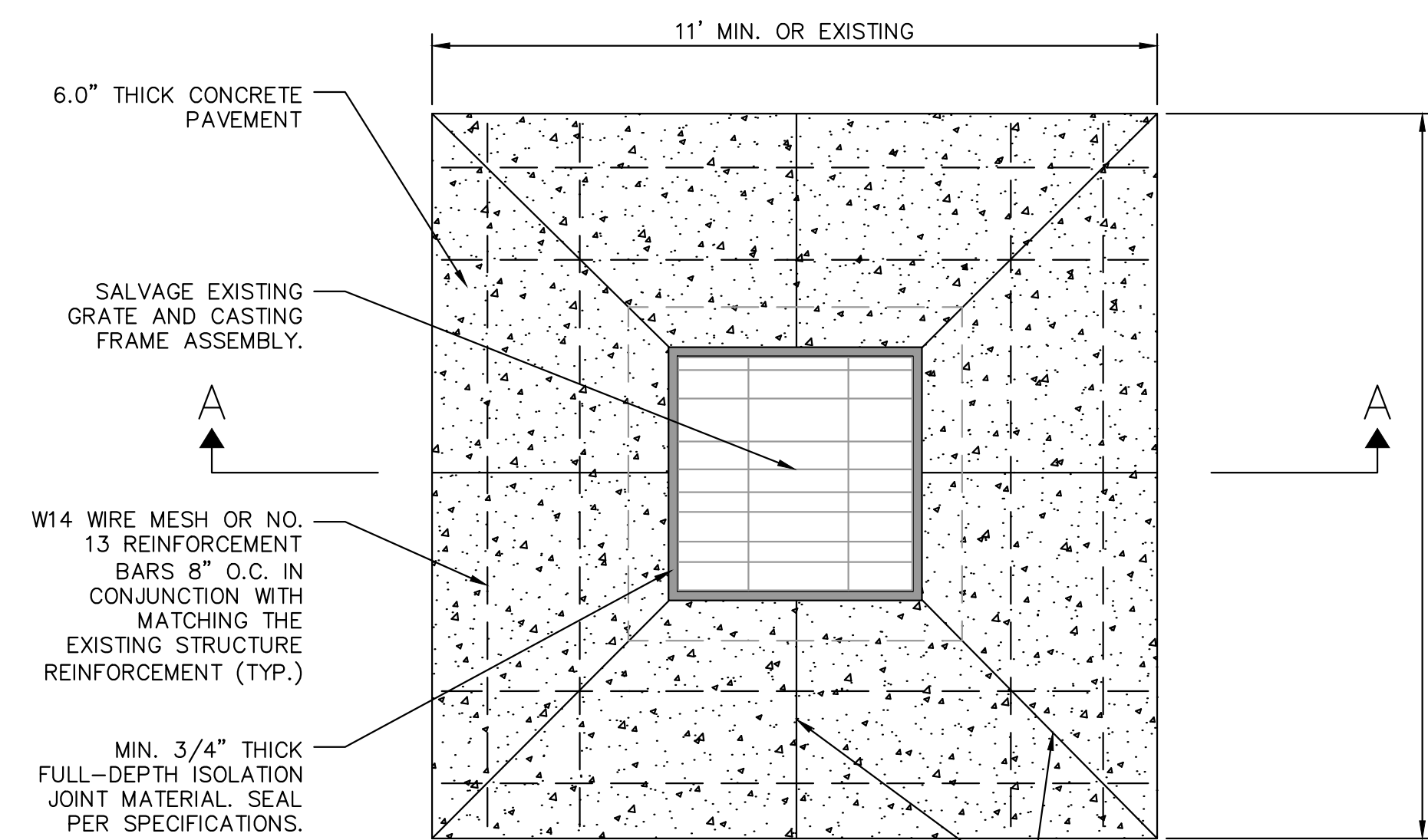


- NOTES:
1. SURFACE COURSE SHALL BE INSTALLED AT SAME TIME AS ADJACENT PAVEMENT REPAIR SURFACE COURSE.
 2. IF AGGREGATE BASE COURSE IS NOT PRESENT CONTRACTOR TO NOTIFY ENGINEER OF RECORD IMMEDIATELY.
 3. AGGREGATE BASE COURSE SHALL BE INSPECTED BY THE TESTING AGENCY, OWNER, OR DESIGN CONSULTANT AFTER ASPHALT REMOVAL AND BASE PREPARATION, PRIOR TO ASPHALT REPLACEMENT.

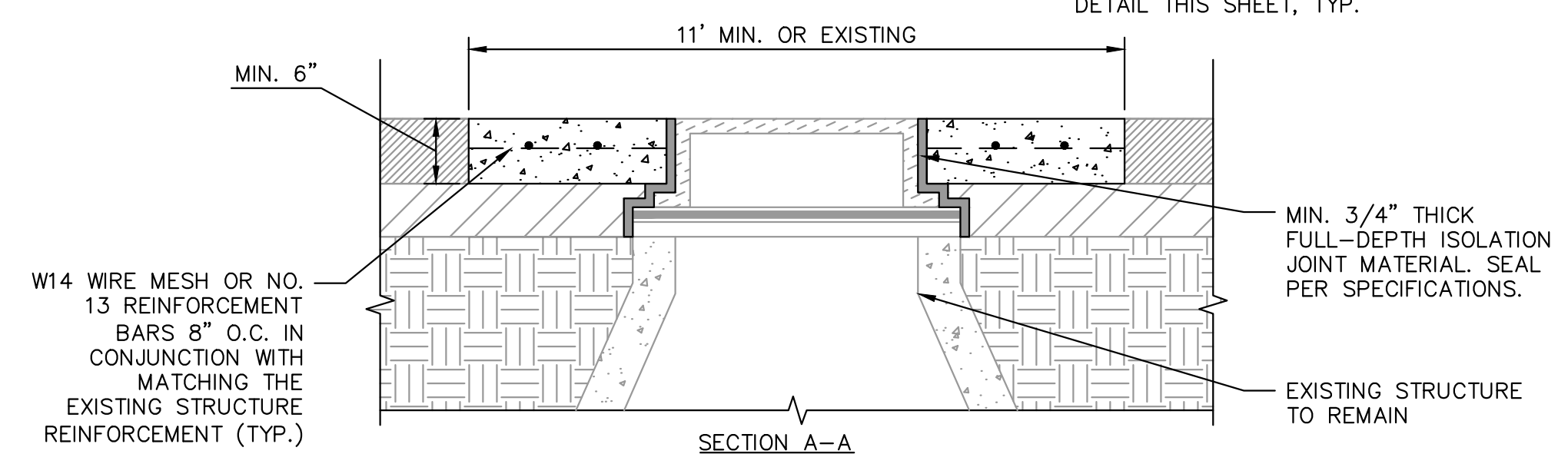
DETAIL

ASPHALT PAVEMENT - 2.5" REMOVE AND REPLACE

AP-RR1



CONTRACTOR SHALL PROVIDE EIGHT (8) TOOLED CONTROL JOINTS IMMEDIATELY ADJACENT TO CASTING WITH SAWED CONTRACTION JOINTS AWAY FROM CASTING TO PREVENT DAMAGE TO CASTING DURING SAWING. CONTRACTOR SHALL SEAL JOINTS PER CONCRETE JOINT SEALANT DETAIL THIS SHEET, TYP.

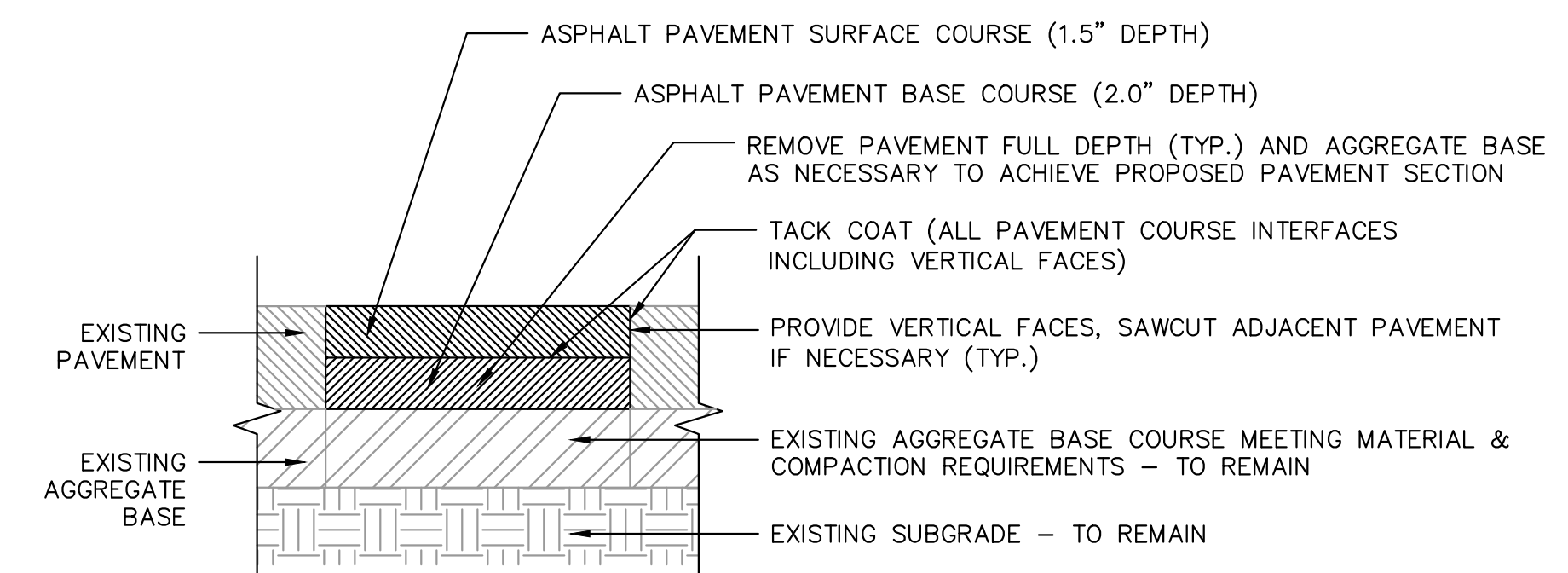


- NOTES:
1. PRIOR TO CONSTRUCTION, CONTRACTOR SHALL VERIFY STRUCTURAL CONDITION OF EXISTING MANHOLE IS SUFFICIENT FOR THE PROPOSED REHABILITATION. IF CONDITIONS IN THE FIELD ARE SUCH THAT PROPOSED REHABILITATION WILL NOT BE POSSIBLE, CONTRACTOR SHALL CONTACT THE ENGINEER/OWNER IMMEDIATELY FOR AN ALTERNATE DESIGN.
 2. CONCRETE COLLAR DIMENSIONS MAY VARY. REFERENCE PLAN SHEETS FOR SPECIFIC COLLAR DIMENSIONS.
 3. REINFORCEMENT SHALL BE ADJUSTED ACCORDINGLY IF DIMENSIONS DIFFER FROM DETAIL.
 4. SAWCUT AND REMOVE EXISTING PAVEMENT FULL-DEPTH WITHIN REPAIR AREA AND EXCAVATE THE AGGREGATE SURROUNDING STRUCTURE TO ACCOMMODATE PROPOSED CROSS SECTIONS.
 5. TAKE CARE TO REMOVE EXISTING CONCRETE STRUCTURE COVER AND SALVAGE EXISTING GRATE AND CASTING ASSEMBLY.
 6. PRIOR TO CONSTRUCTION, CONTRACTOR SHALL SET CASTING AND ENSURE THAT ANY REVISED PAVEMENT SLOPES RESULTING FROM ADJUSTMENT OF STRUCTURE WITHIN REPAIR AREAS MAINTAIN POSITIVE DRAINAGE.
 7. COMPACTED AGGREGATE BACKFILL AROUND STRUCTURE PRIOR TO PLACEMENT OF CONCRETE PER SPECIFICATIONS.
 8. TUCK POINT INTERIOR OF STRUCTURE WITH MORTAR UP TO 3' BELOW CASTING

DETAIL

CONCRETE PAVEMENT - REMOVE AND REPLACE CONCRETE COLLAR

CP-CC



- NOTES:
1. SURFACE COURSE SHALL BE INSTALLED AT SAME TIME AS ADJACENT PAVEMENT REPAIR SURFACE COURSE.
 2. IF AGGREGATE BASE COURSE IS NOT PRESENT CONTRACTOR TO NOTIFY ENGINEER OF RECORD IMMEDIATELY.
 3. AGGREGATE BASE COURSE SHALL BE INSPECTED BY THE TESTING AGENCY, OWNER, OR DESIGN CONSULTANT AFTER ASPHALT REMOVAL AND BASE PREPARATION, PRIOR TO ASPHALT REPLACEMENT.

DETAIL

ASPHALT PAVEMENT - 3.5" REMOVE AND REPLACE

AP-RR2

EROSION CONTROL	07/07/2014
NO.	
REVISIONS	
DATE	

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KHA PROJECT	168719048
DATE	04/04/2020
SCALE	AS SHOWN
DESIGNED BY	ATO
DRAWN BY	ATO
CHECKED BY	JHP

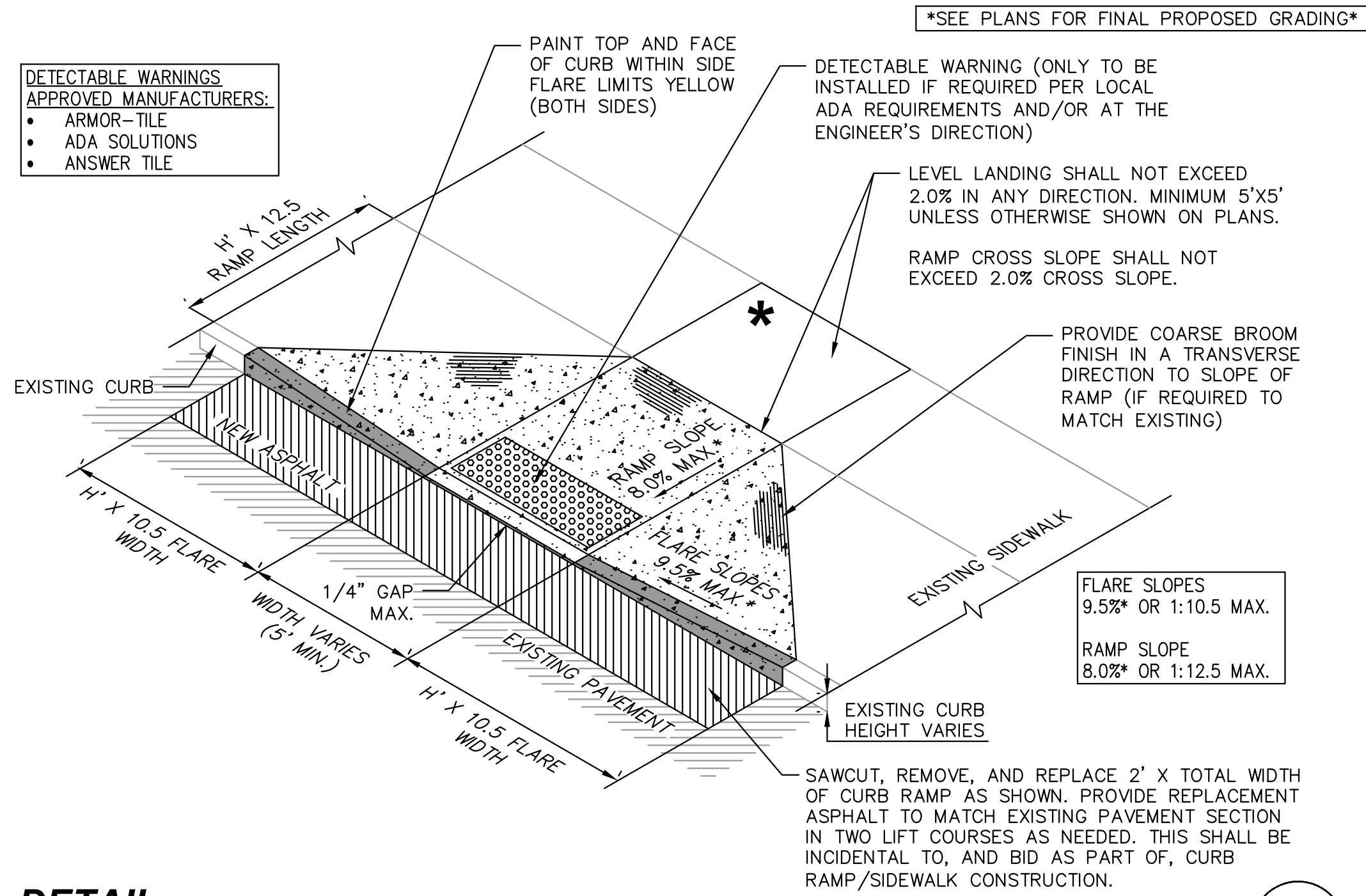
CONSTRUCTION
DETAILS

THE CENTER AT
HOBBS BROOK
STURBRIDGE, MA

SHEET NUMBER

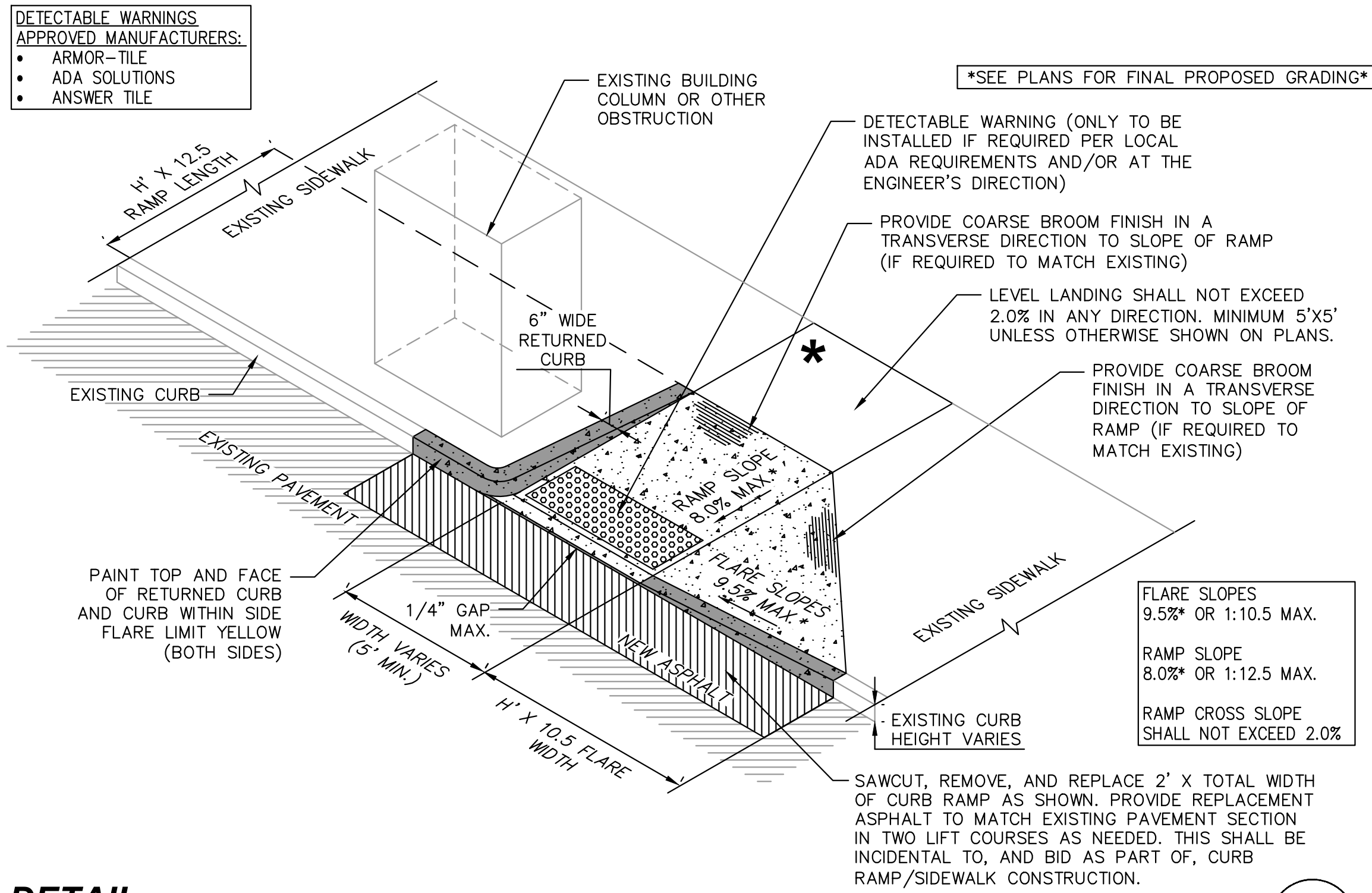
C2-00

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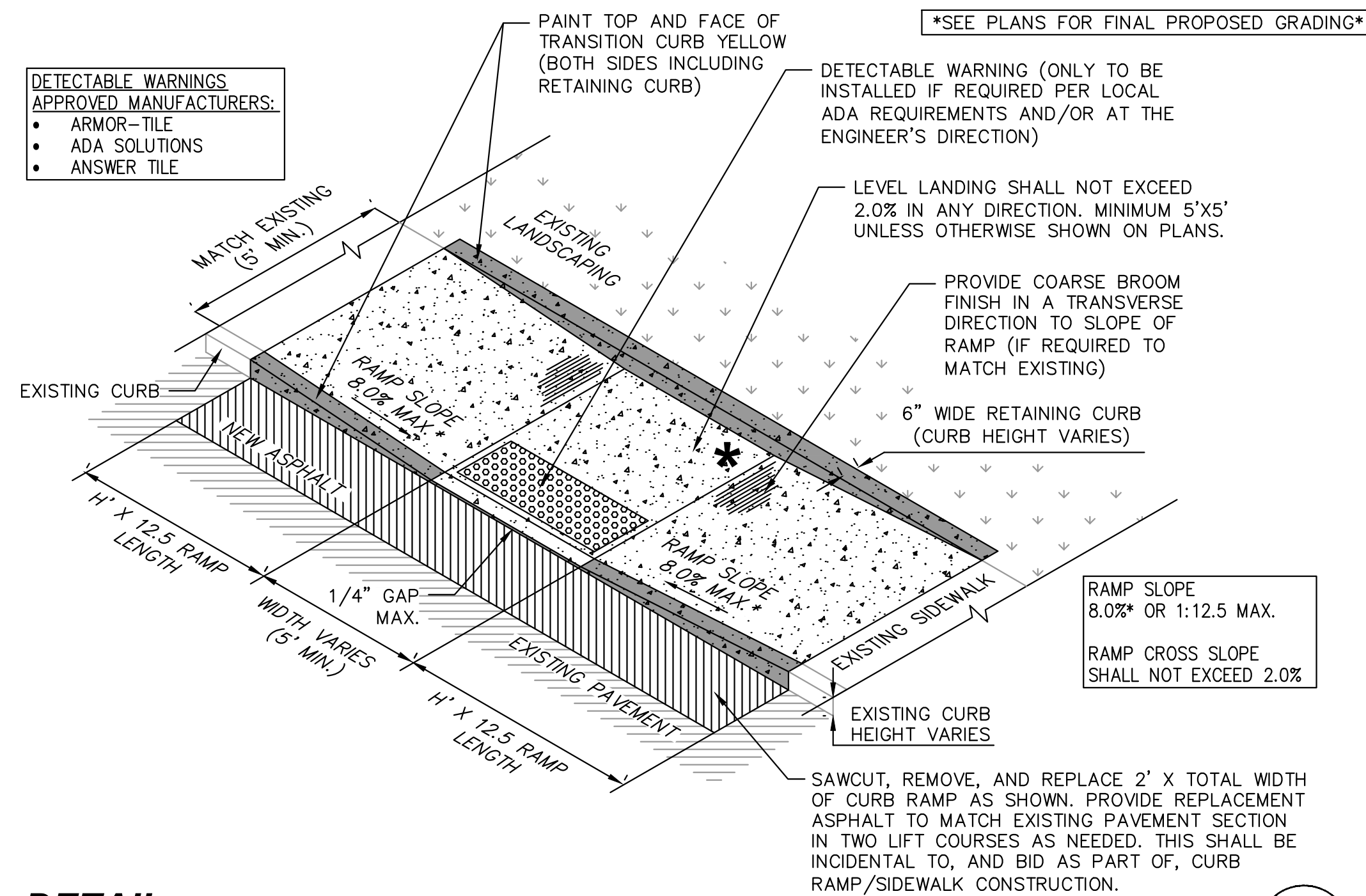
DETAIL
CONCRETE IMPROVEMENTS - CURB RAMP CONSTRUCTION

CP-CR



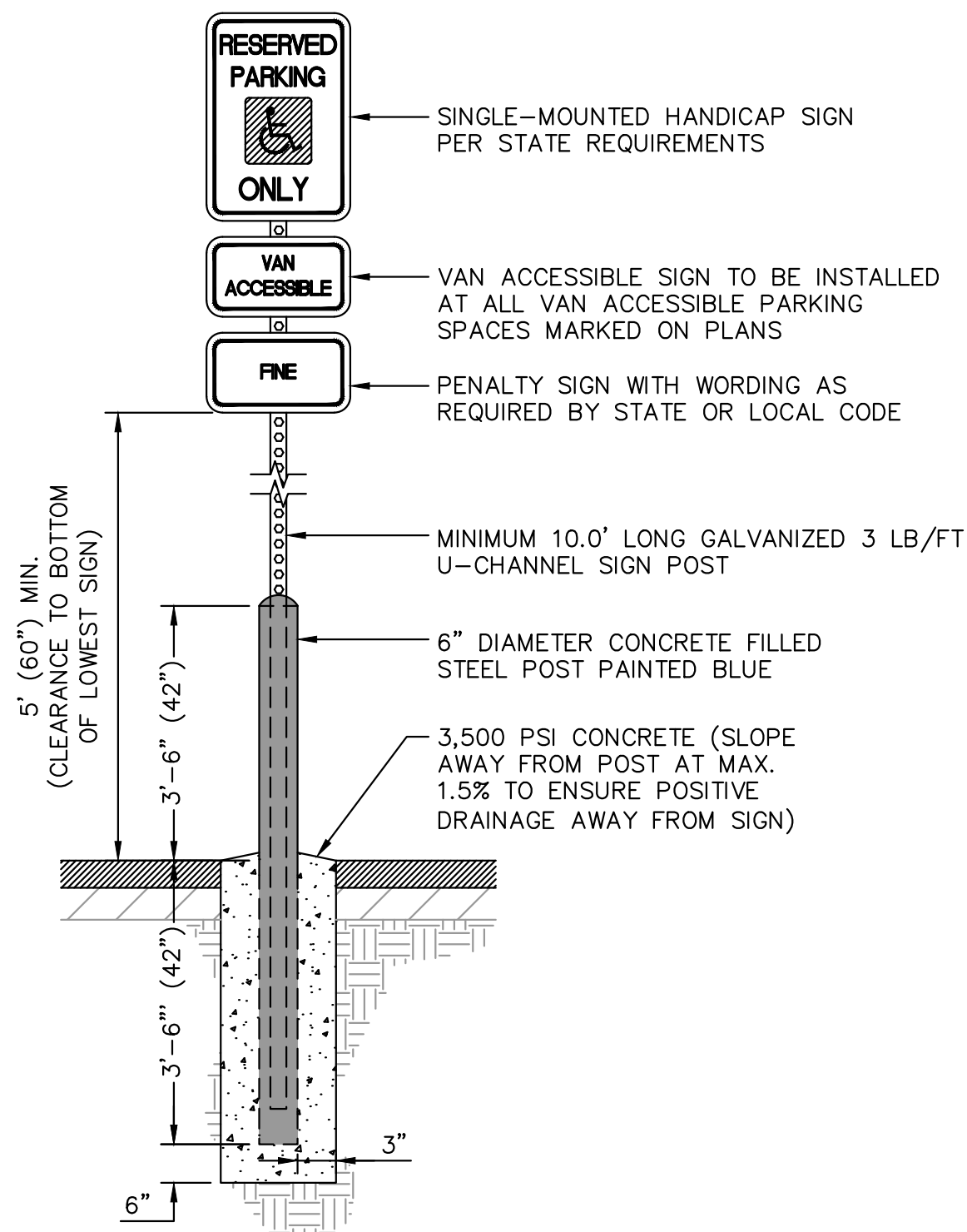
DETAIL
CONCRETE IMPROVEMENTS - CURB RAMP CONSTRUCTION (OBSTRUCTION)

CP-OCR



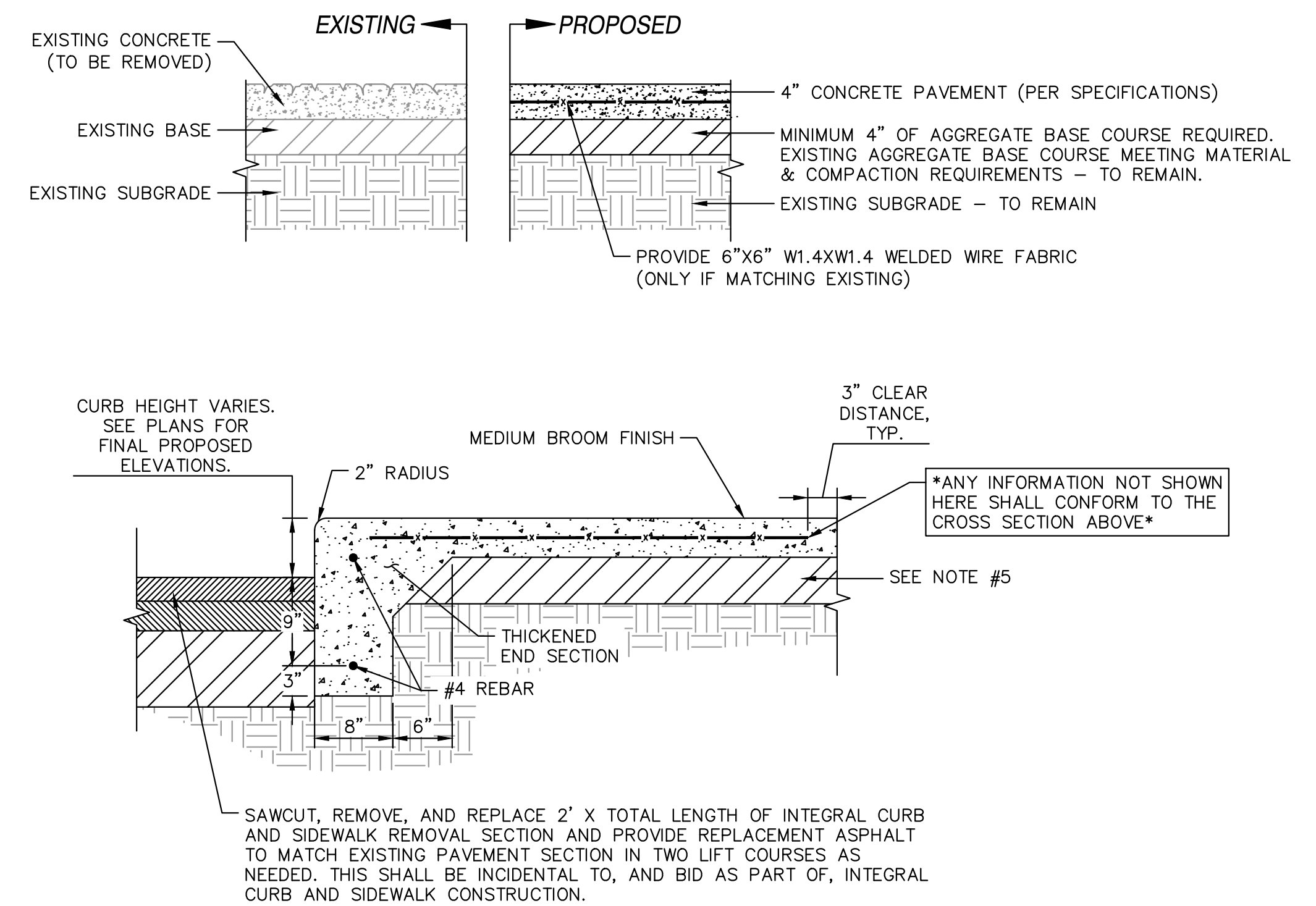
DETAIL
CONCRETE IMPROVEMENTS - CURB RAMP CONSTRUCTION (PARALLEL)

CP-PCR



DETAIL
MISCELLANEOUS IMPROVEMENTS - ADA SIGN AND POST WITH BOLLARD

MI-SPB



DETAIL
CONCRETE PAVEMENT - REMOVE, REGRADE, AND REPLACE INTEGRAL CURB AND SIDEWALK

CP-SWI

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CONSTRUCTION DETAILS

**THE CENTER AT
HOBBS BROOK
STURBRIDGE, MA**

SHEET NUMBER
C2-01

NO.	REVISIONS	DATE
1		

NO.	REVISIONS	DATE
1		

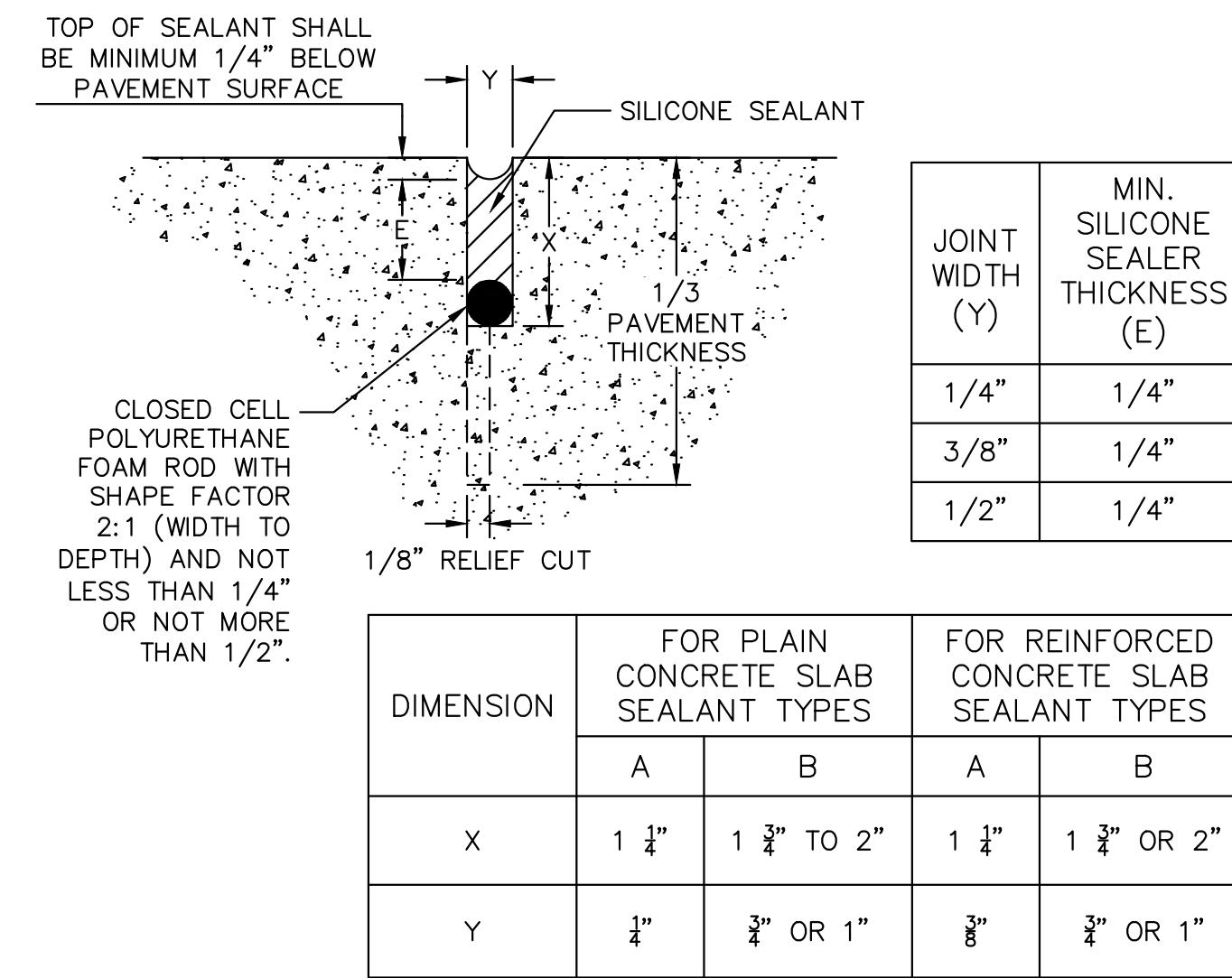
DATE	DESIGNED BY	DRAWN BY	CHECKED BY
04/04/2020	ATO	ATO	JHP

PROJECT NO.	DATE	SCALE	DESIGNED BY	DRAWN BY	CHECKED BY
168719048	04/04/2020	AS SHOWN	ATO	ATO	JHP

NO.	REVISIONS	DATE
1		

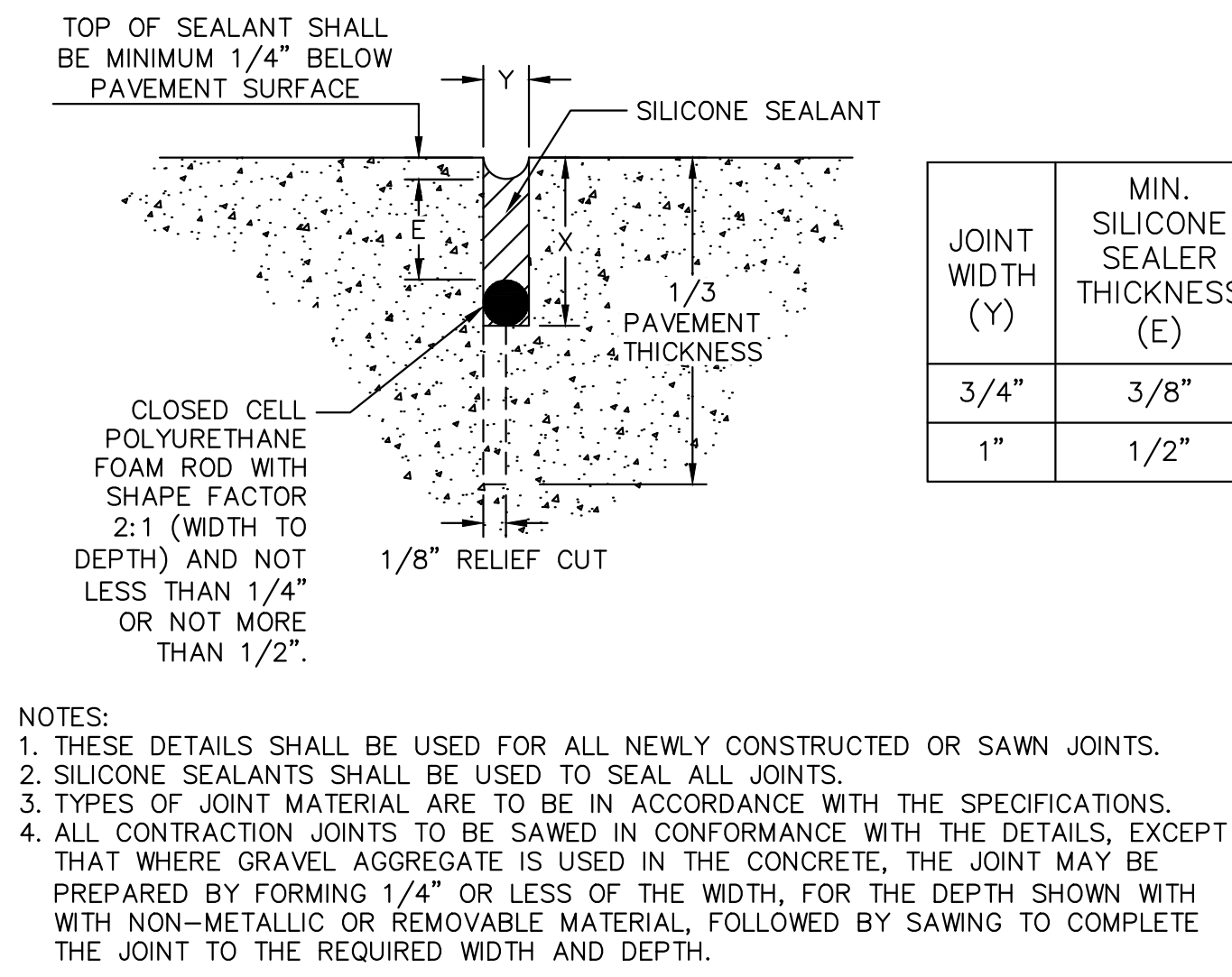
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CONTRACTION JOINTS (TYPE A SILICONE)

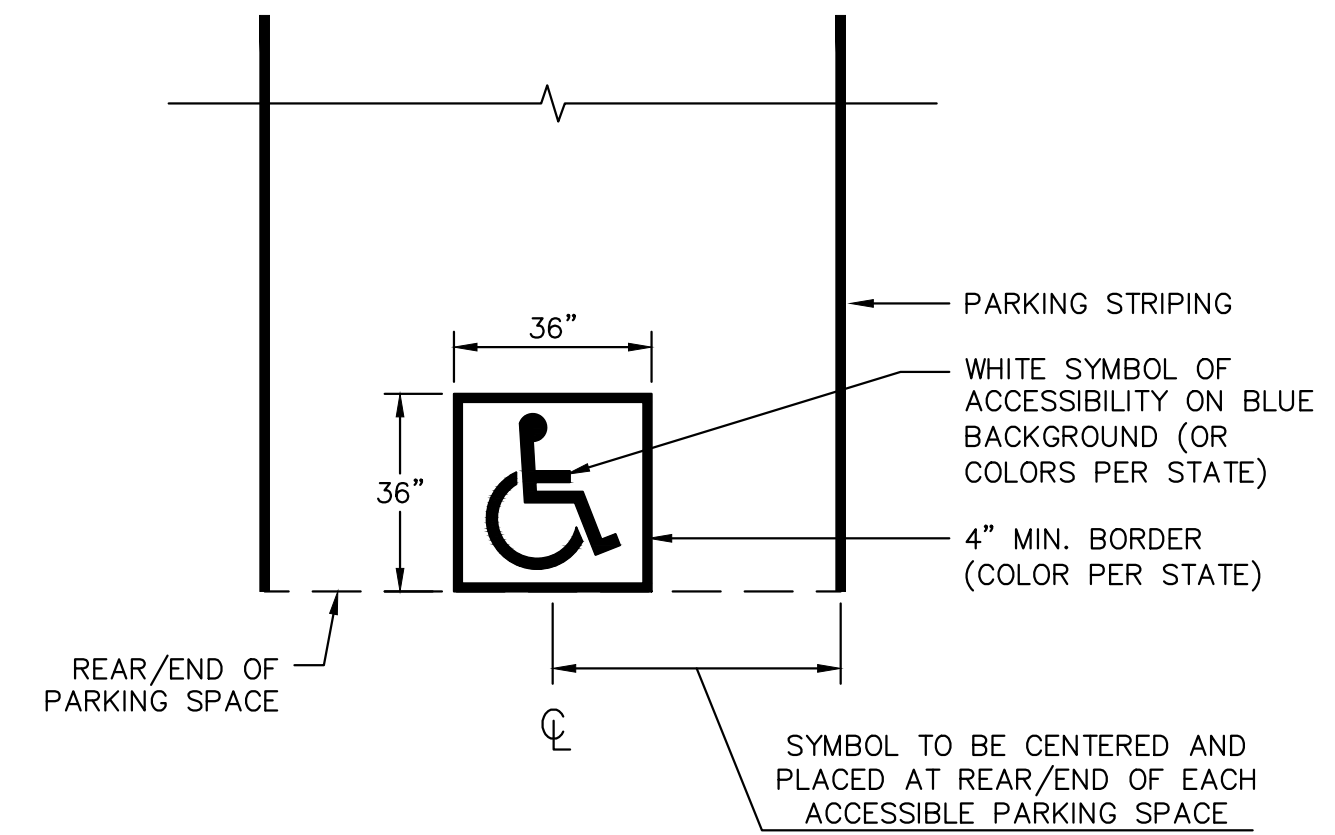


DETAIL
CONCRETE JOINT SEALANT

EXPANSION JOINTS (TYPE B SILICONE)

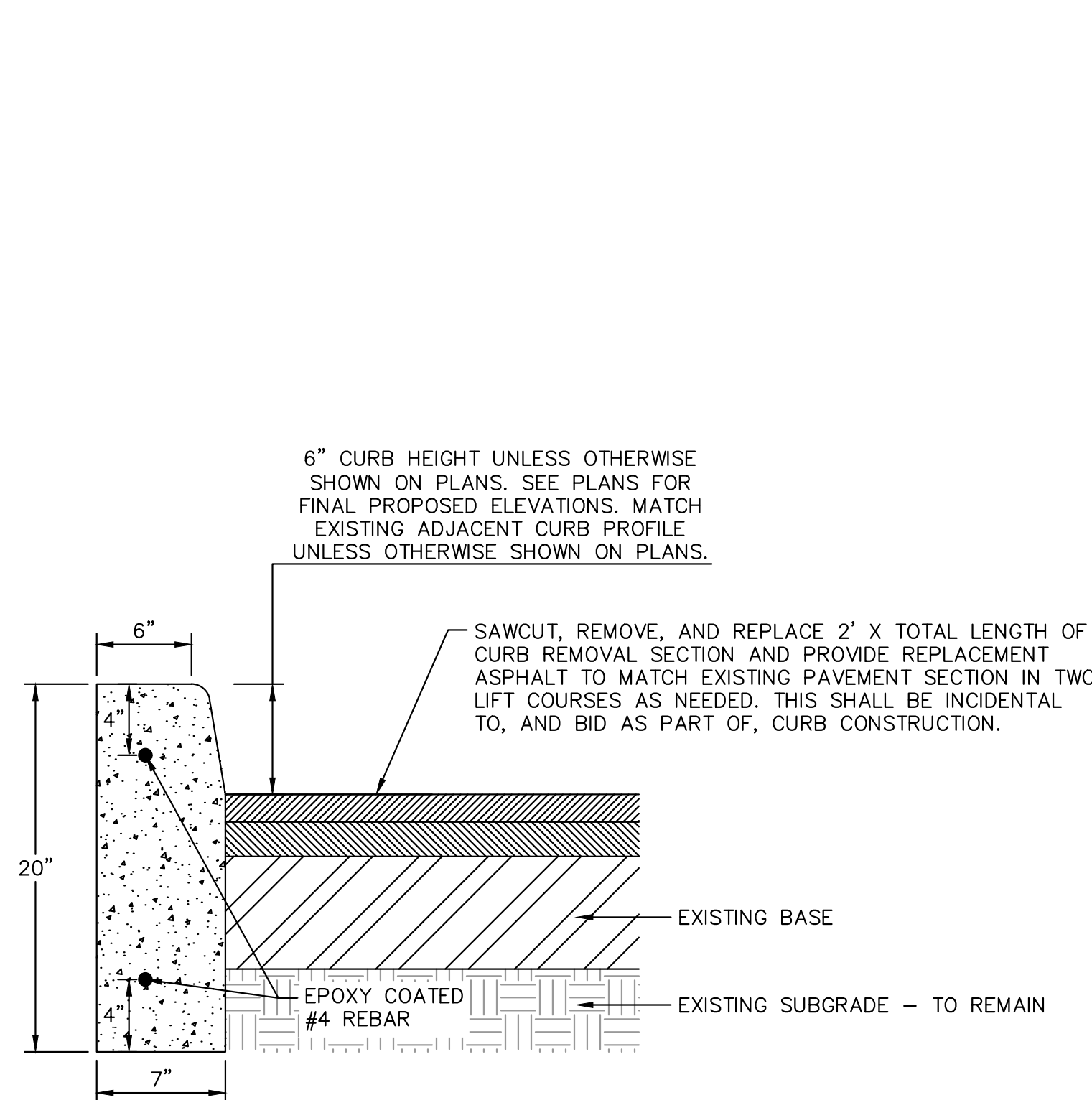


NOTES:
 1. THESE DETAILS SHALL BE USED FOR ALL NEWLY CONSTRUCTED OR SAWN JOINTS.
 2. SILICONE SEALANTS SHALL BE USED TO SEAL ALL JOINTS.
 3. TYPES OF JOINT MATERIAL ARE TO BE IN ACCORDANCE WITH THE SPECIFICATIONS.
 4. ALL CONTRACTION JOINTS TO BE SAWN IN CONFORMANCE WITH THE DETAILS, EXCEPT THAT WHERE GRAVEL AGGREGATE IS USED IN THE CONCRETE, THE JOINT MAY BE PREPARED BY FORMING 1/4" OR LESS OF THE WIDTH, FOR THE DEPTH SHOWN WITH WITH NON-METALLIC OR REMOVABLE MATERIAL, FOLLOWED BY SAWING TO COMPLETE THE JOINT TO THE REQUIRED WIDTH AND DEPTH.



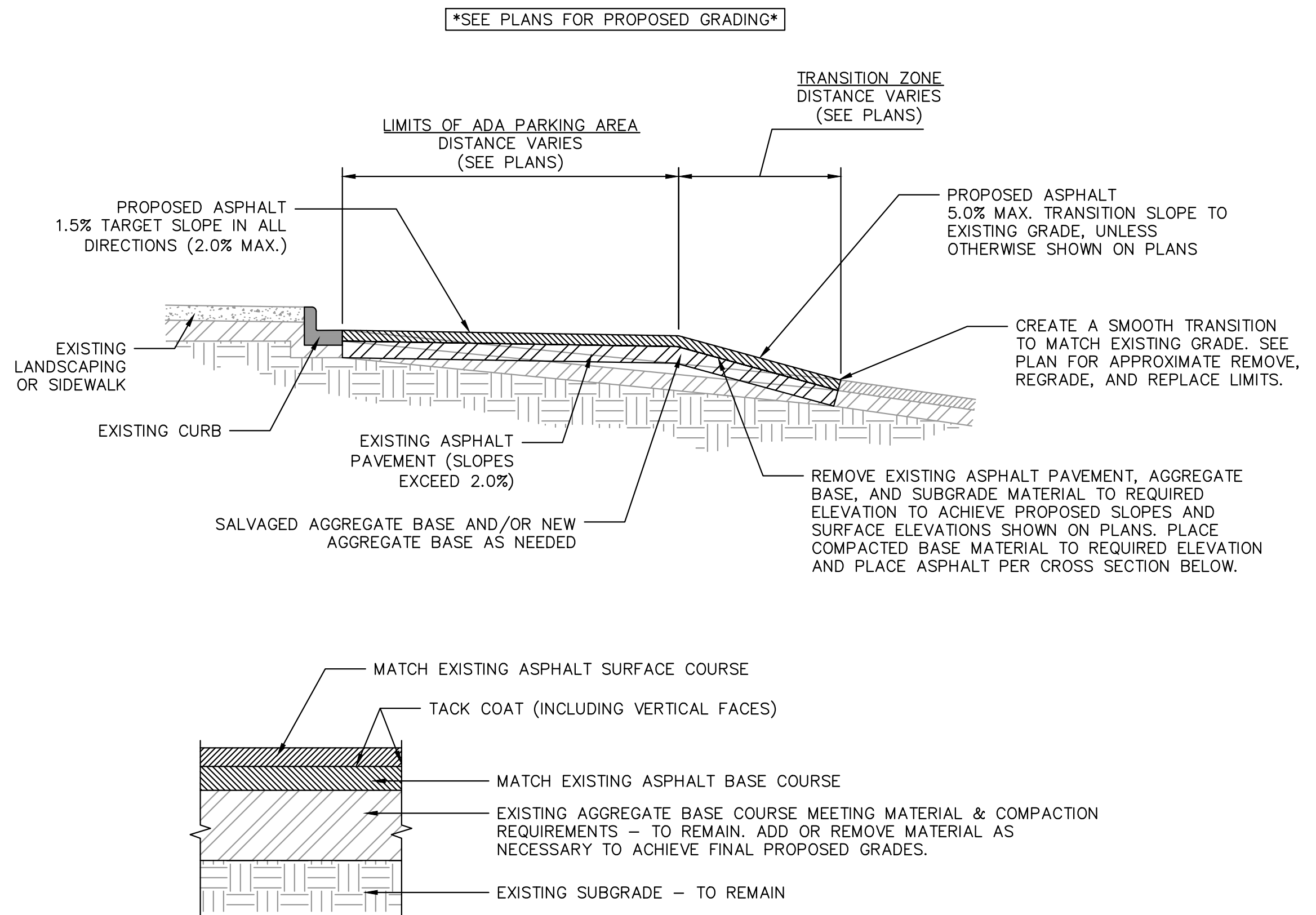
NOTES:
 1. PAVEMENT MARKING STROKE WIDTH = 2" MINIMUM
 2. CONTRACTOR SHALL ONLY PAINT IN SYMBOL OF ACCESSIBILITY IN PARKING SPACES DESIGNATED WITH ADA PARKING SIGNS.

DETAIL
MISCELLANEOUS IMPROVEMENTS - ADA SYMBOL WITH BACKGROUND



NOTES:
 1. DETAIL SHOWN FOR REFERENCE ONLY. FIELD CONSTRUCTION SHALL MATCH EXISTING CURB TYPE, INCLUDING DIMENSIONS AND REINFORCEMENT, AS WELL AS ELEVATION, UNLESS OTHERWISE SPECIFICALLY SHOWN ON PLANS.
 2. VERTICAL SAWCUT SHALL BE MADE AT ALL LIMITS OF REMOVAL TO CREATE A CLEAN EDGE.
 3. GRADING, ADDITIONAL AGGREGATE/CONCRETE REQUIRED TO MATCH EXISTING CROSS SECTION GRADES, AND SEEDING OF ADJACENT LANDSCAPE AREA SHALL BE INCIDENTAL TO, AND BID AS PART OF, CURB CONSTRUCTION.

DETAIL
CONCRETE PAVEMENT - REMOVE AND REPLACE VERTICAL CURB



NOTES:
 1. SURFACE COURSE SHALL BE INSTALLED AT THE SAME TIME AS ADJACENT PAVEMENT REPAIR SURFACE COURSES.
 2. IF AGGREGATE BASE COURSE IS NOT PRESENT CONTRACTOR TO NOTIFY ENGINEER OF RECORD IMMEDIATELY.
 3. THE AGGREGATE BASE COURSE SHALL BE INSPECTED BY THE TESTING AGENCY, OWNER OR DESIGN CONSULTANT AFTER ASPHALT REMOVAL AND BASE PREPARATION, PRIOR TO ASPHALT REPLACEMENT.

DETAIL
ASPHALT PAVEMENT - REMOVE, REGRADE, AND REPLACE

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KHA PROJECT 168719048
 DATE 04/04/2020
 SCALE AS SHOWN
 DESIGNED BY ATO
 DRAWN BY ATO
 CHECKED BY JHP

CONSTRUCTION DETAILS

THE CENTER AT HOBBS BROOK STURBRIDGE, MA

SHEET NUMBER
C2-02

NO.	REVISIONS	DATE	BY
1	EROSION CONTROL	07/07/20 WAD	

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GENERAL NOTES

- 1. ALL WORK SHALL COMPLY WITH THE REQUIREMENTS OF THE PROJECT PLANS AND SPECIFICATIONS. WHERE THE CONTRACTOR DETERMINES A CONFLICT EXISTS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND ALLOW THE ENGINEER SUFFICIENT TIME TO ADDRESS THE CONFLICT. FAILURE TO NOTIFY THE ENGINEER WILL NOT CAUSE THE OWNER ADDITIONAL EXPENSES FOR NOTED CONFLICTS.
- 2. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH CURRENT O.S.H.A. CODES AND STANDARDS. NOTHING INDICATED ON THESE PLANS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH APPROPRIATE SAFETY REGULATIONS.
- 3. ALL ACCESSIBLE SITE FEATURES SHALL BE CONSTRUCTED TO MEET THE CURRENT STANDARDS SET FORTH IN THE LATEST AMERICANS WITH DISABILITIES ACT (ADA).
- 4. THE CONTRACTOR SHALL REFER TO AND COMPLY WITH THE PROJECT BID DOCUMENTS FOR ALL WORK. THE CONTRACTOR SHOULD BE FAMILIAR WITH THE EXISTING SITE CONDITIONS AND THE PROPOSED PLANS. ANY EXISTING CONDITION FOUND IN CONFLICT WITH THE PROPOSED SCOPE OF WORK MUST BE IMMEDIATELY REPORTED TO THE ENGINEER AND OWNER TO ADDRESS.
- 5. THE CONTRACTOR SHALL VERIFY THAT THE DESIGN PLANS AND SPECIFICATIONS ARE CURRENT AND HAVE BEEN APPROVED BY THE OWNER AND ALL APPLICABLE PERMIT-ISSUING AGENCIES. ALL ITEMS CONSTRUCTED BY THE CONTRACTOR PRIOR TO RECEIVING FINAL APPROVAL AND/OR PERMITS THAT REQUIRE ADJUSTMENT OR REPLACEMENT, SHALL BE COMPLETED AT THE SOLE EXPENSE OF THE CONTRACTOR.
- 6. THE CONTRACTOR IS CAUTIONED THAT THE EXISTING INFORMATION SHOWN ON THESE PLANS ARE BASED ON CAD FILES AND NOT FROM FIELD MEASUREMENTS. THE ENGINEER DOES NOT GUARANTEE THE COMPLETENESS OR ACCURACY OF THE INFORMATION SHOWN ON THE PLANS. THE CONTRACTOR WILL BE RESPONSIBLE FOR FIELD VERIFYING EXISTING SITE CONDITIONS.
- 7. ALL CONSTRUCTION OPERATIONS MUST BE DONE IN SUCH A MANNER AS TO PROTECT ALL ADJACENT EXISTING BUILDINGS, PAVEMENTS, LANDSCAPING AND OTHER SITE ELEMENTS. THIS INCLUDES USING SMALLER EQUIPMENT OR CONTROLLING ACCESS ROUTES FOR EQUIPMENT TO ELIMINATE DAMAGE TO SITE PAVEMENTS. ANY EXISTING ELEMENTS DAMAGED DURING CONSTRUCTION MUST BE REPAIRED TO THE OWNER'S SATISFACTION AT THE SOLE EXPENSE OF THE CONTRACTOR.
- 8. THE CONTRACTOR SHALL PROVIDE FOR THE CONTINUOUS OPERATION OF EXISTING FACILITIES WITHOUT INTERRUPTION DURING CONSTRUCTION UNLESS SPECIFICALLY AUTHORIZED OTHERWISE BY THE OWNER.
- 9. THE CONTRACTOR'S STAGING AND MATERIAL STORAGE AREAS MUST BE COORDINATED WITH THE OWNER. ALL SUCH AREAS AND CONSTRUCTION METHODS MUST BE DONE IN SUCH A MANNER AS TO AVOID INTERFERENCE WITH THE OWNER'S OPERATIONS AND CUSTOMER ACCESS.
- 10. THE CONTRACTOR SHALL MAKE APPLICATION FOR, OBTAIN AND HAVE IN POSSESSION ALL NECESSARY PERMITS REQUIRED TO COMPLETE THE WORK PRIOR TO THE START OF CONSTRUCTION.
- 11. THE CONTRACTOR SHALL CONFINE ACTIVITIES TO THE PROJECT SITE UNDER DEVELOPMENT, EXISTING RIGHTS-OF-WAY, AND EASEMENTS AS APPLICABLE. THE CONTRACTOR SHALL NOT TRESPASS UPON ANY OTHER PRIVATE PROPERTY WITHOUT THE WRITTEN CONSENT OF THE RESPECTIVE PROPERTY OWNER.
- 12. THE CONTRACTOR SHALL CLEAN UP ALL DEBRIS AND MATERIALS RESULTING FROM OPERATIONS AND RESTORE ALL SURFACES, STRUCTURES, DITCHES AND OTHER SITE ELEMENTS TO THEIR ORIGINAL CONDITION AND THE SATISFACTION OF THE OWNER.
- 13. THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING A LICENSED MATERIALS TESTING AGENCY, SUPPORTED BY A REGISTERED PROFESSIONAL ENGINEER, TO PROVIDE ONSITE INSPECTION AND VERIFY IN THE FIELD THAT ALL MIX DESIGNS ARE IN COMPLIANCE WITH THE PLANS AND SPECIFICATIONS AND THAT ALL BACKFILL AND PAVEMENT HAS BEEN PLACED WITH THE PROPER NUMBER OF LAYERS AND THICKNESS AND COMPACTED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS.
- 14. EMPLOYMENT OF A MATERIALS TESTING AGENCY SHALL NOT RELIEVE THE CONTRACTOR OF OBLIGATIONS TO PERFORM THE WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL COORDINATE AND PROVIDE 48 HOUR NOTICE TO THE MATERIALS TESTING AGENCY PER SPECIFICATION FOR INSPECTION OF WORK.
- 15. REPORTS, CERTIFICATIONS, MATERIAL CHECKS AND FIELD TESTS SHALL BE COMPLETED DAILY AND A COPY OF THE RESULTS SHOULD BE EMAILED TO THE PROJECT ENGINEER DAILY.
- 16. IF IN THE OPINION OF THE ENGINEER, THE WORK DOES NOT MEET THE TECHNICAL OR DESIGN REQUIREMENTS STIPULATED FOR THE WORK, THE CONTRACTOR SHALL MAKE ALL NECESSARY ADJUSTMENTS FOR APPROVAL AT THEIR SOLE EXPENSE. THE CONTRACTOR SHALL MAKE NO DEVIATIONS FROM THE CONTRACT DOCUMENTS WITHOUT SPECIFIC WRITTEN APPROVAL OF THE ENGINEER OR OWNER.
- 17. THE CONTRACTOR SHALL CERTIFY THAT THE PAVEMENT AND ALL RELATED WORK HAS BEEN INSTALLED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. CERTIFICATIONS SHALL BE EMAILED TO THE PROJECT ENGINEER. IF REQUIRED, THE CONTRACTOR SHALL BE RESPONSIBLE TO MAKE ANY CHANGES IN THE FIELD FOR COMPLIANCE WITH THE CONTRACT DOCUMENTS AT HIS SOLE EXPENSE.
- 18. THE CONTRACTOR SHALL BE RESPONSIBLE TO CERTIFY THAT PROPER SUBGRADE AND TRENCH BACKFILL COMPACTION HAS BEEN OBTAINED BY THE SUBCONTRACTORS UNDER THEIR SUPERVISION.
- 19. IF THE CONTRACTOR'S OPERATIONS ARE SHOWN TO NEGATIVELY IMPACT THE OWNER'S USE OF THE SITE, THE OWNER AND/OR THE ENGINEER SHALL PROVIDE 24-HOUR NOTICE TO THE CONTRACTOR TO MODIFY THEIR OPERATIONS TO THE OWNER'S SATISFACTION.

SITE INFRASTRUCTURE NOTES

- 1. THE CONTRACTOR SHALL NOTIFY ALL UTILITY OWNERS BY CALLING 811 AT LEAST 72 HOURS BEFORE ANY EXCAVATION (EXCLUDING WEEKENDS AND HOLIDAYS) TO REQUEST EXACT FIELD LOCATION AND STAKING OF UTILITIES. IF NO NOTIFICATION IS GIVEN AND DAMAGE RESULTS, SAID DAMAGE WILL BE REPAIRED AT THE SOLE EXPENSE OF THE CONTRACTOR. THE CONTRACTOR SHALL NOTIFY THE OWNER OF THE UTILITY IMMEDIATELY UPON BREAK OR DAMAGE TO ANY UTILITY LINE OR APPURTENANCE THEREOF. WHEN HIS OPERATIONS MAY REQUIRE AN INTERRUPTION OF THE UTILITY SERVICE OR POTENTIALLY CAUSE DAMAGE TO THE UTILITY INVOLVED, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER PRIOR TO CONSTRUCTION.
- 2. IF EXISTING UTILITY LINES ARE ENCOUNTERED THAT CONFLICT IN LOCATION WITH PROPOSED CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY SO THAT THE CONFLICT MAY BE RESOLVED.
- 3. ALL UTILITY MANHOLE COVERS, VALVES, BOXES, FITTINGS AND/OR OTHER UTILITY APPURTENANCES THAT ARE AT THE SURFACE OF PAVEMENTS TO BE REPLACED OR REPAIRED SHALL BE ADJUSTED FLUSH WITH THE NEW SURFACE SO AS NOT TO BE COVERED WITH PAVEMENT OR CAUSE A TRIPPING HAZARD.
- 4. IN THE EVENT THAT EXISTING UTILITY SERVICES ARE TO BE INTERRUPTED OR IMPACTED BY NECESSARY CONSTRUCTION OPERATIONS, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO GIVE PRIOR NOTIFICATION TO THE RESPECTIVE OWNER/UTILITY COMPANY AND ALL SERVICE USERS TO BE AFFECTED BY THE SERVICE INTERRUPTION. ALL SERVICE INTERRUPTIONS SHALL BE PERFORMED AT OFF-PEAK HOURS, WHEN POSSIBLE, TO MINIMIZE ANY INCONVENIENCE TO THE AFFECTED PARTIES.
- 5. ALL EXISTING UTILITIES AND SITE INFRASTRUCTURE, INCLUDING BUT NOT LIMITED TO, LIGHT POLES, BOLLARDS, WATER MAINS, FIELD TILES, SEWERS, AND UTILITY SERVICE LINES SHALL BE PROTECTED BY THE CONTRACTOR DURING CONSTRUCTION. DAMAGED INFRASTRUCTURE SHALL BE REPAIRED TO ITS ORIGINAL CONDITION AT THE SOLE EXPENSE OF THE CONTRACTOR.

SITE ACCESS/TRAFFIC CONTROL NOTES

- 1. ALL TRAFFIC CONTROLS ON THIS PROJECT SHALL ADHERE TO THE REQUIREMENTS OF THE AGENCY HAVING JURISDICTION. ALL SIGNS, SUPPORTS, BARRICADES AND OTHER REQUIRED MATERIALS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), CURRENT EDITION, AND SHALL BE PROVIDED AND INSTALLED BY THE CONTRACTOR.
- 2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ALL REQUIRED PLANS TO RECEIVE APPROVAL FROM THE LOCAL GOVERNING AGENCY FOR THE PROPER INSTALLATION OF NECESSARY TRAFFIC CONTROLS AND WORK AREA PROTECTION AS APPLICABLE.
- 3. TRAFFIC CONTROLS SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF ANY WORK REQUIRING SUCH MATERIALS. THEY SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION AND SHALL REMAIN IN PLACE UNTIL THE CONCLUSION OF WORK.
- 4. PROPER SIGNING AND BARRICADING SHALL BE PROVIDED BY THE CONTRACTOR. IF IN THE OPINION OF THE ENGINEER OR OWNER, ADDITIONAL SIGNS AND BARRICADES, INCLUDING LANTERNS AND/OR HIGH RISE WARNING DEVICES, ARE NEEDED TO MAINTAIN OPERATIONS, SUCH CONTROLS SHALL BE PROVIDED AT THE SOLE EXPENSE OF THE CONTRACTOR.
- 5. THE CONTRACTOR SHALL NOTIFY THE OWNER AND ENGINEER DAILY AND WEEKLY OF THE PROJECT SCHEDULE. ACCESS SHALL BE MAINTAINED AT ALL TIMES TO THE PROPERTIES ADJOINING THE CONSTRUCTION SITES, INCLUDING ROADWAYS PERMANENTLY OR TEMPORARILY CLOSED TO THIRCE DEPARTMENTS, ACCESS TO PRIVATE PROPERTIES MAY BE RESTRICTED TEMPORARILY FOR THE INSTALLATION OF A SPECIFIC UTILITY OR CONSTRUCTION ACTIVITY. IN THIS EVENT, THE PROPERTY OWNER SHALL BE CONTACTED BY THE CONTRACTOR AT LEAST TWENTY-FOUR (24) HOURS IN ADVANCE OF THE RESTRICTION AND SHALL BE GIVEN THE TIME AND DURATION OF THE RESTRICTION, IN NO EVENT SHALL ACCESS BE DENIED OVERNIGHT OR FOR MORE THAN EIGHT (8) CONSECUTIVE HOURS. IF TEMPORARY ACCESS IS AFFORDED NEAR EXCAVATIONS, SUCH ACCESS SHALL BE CLEARLY DELINEATED AND ADEQUATE MEASURES TAKEN AS REQUIRED TO INSURE ADEQUATE, SAFE ACCESS. ACCESS TO PROPERTIES SHALL BE MAINTAINED AT ALL TIMES, WHERE MULTIPLE DRIVEWAYS ARE PRESENT, AT LEAST ONE SHALL REMAIN OPEN FOR ACCESS AT ALL TIMES. WHERE ONLY ONE DRIVEWAY IS PRESENT, WORK SHALL PROGRESS SUCH THAT ONLY ONE-HALF OF THE DRIVEWAY IS RESTRICTED OR AN ALTERNATE ACCESS SHALL BE PROVIDED IF THE ENTIRE SECTION IS CLOSED. PARTIAL CLOSURE SCHEDULES SHALL BE SUBMITTED TO THE BUSINESS PROPRIETOR AT LEAST TWENTY-FOUR (24) HOURS IN ADVANCE OF THE STARTING TIME AND FOR THE DURATION OF CLOSURE.
- 6. THE CONTRACTOR SHALL BE RESPONSIBLE TO NOTIFY ALL GOVERNING AGENCIES AND EMERGENCY RESPONSE PROVIDERS, INCLUDING FIRE AND POLICE DEPARTMENTS, BY PHONE OR IN WRITING, BEFORE THE BEGINNING OF HIS OPERATIONS SO THAT THESE AGENCIES MAY PLAN THEIR OPERATIONS IN CONSIDERATION OF THE CONSTRUCTION ZONE AND RELATED ACTIVITIES.
- 7. FIRE DEPARTMENT ACCESS SHALL BE MAINTAINED AT ALL TIMES.
- 8. THE ENGINEER, OWNER, CITY AND STATE SHALL NOT BE HELD LIABLE FOR ANY CLAIMS RESULTING FROM ACCIDENTS OR DAMAGES CAUSED BY THE CONTRACTOR'S FAILURE TO COMPLY WITH TRAFFIC AND PUBLIC SAFETY REGULATIONS DURING THE CONSTRUCTION PERIOD.

GRADING NOTES

- 1. THE CONTRACTOR SHALL CONFIRM THAT THE FINAL PAVEMENT GRADES WILL NOT CREATE A PONDING WATER CONDITION (I.E. A LOW SLOP OR PAVEMENT SLOPES LESS THAN 1%) OR SLOPES IN EXCESS OF 5%. THE CONTRACTOR SHALL ADVISE ENGINEER IMMEDIATELY IF ONE OF THESE SITUATIONS WILL OCCUR.
- 2. "SKIN PATCHING" WILL NOT BE PERMITTED FOR THE REPAIR OF WATER PONDING AREAS. AT A MINIMUM, A 1" THICK MILL AND REPLACE SHOULD BE USED FOR THE REPAIR OF THESE AREAS AS NECESSARY.
- 3. THE CONTRACTOR IS RESPONSIBLE FOR CONTROLLING STORM WATER RUNOFF DURING CONSTRUCTION OPERATIONS. OF PARTICULAR CONCERN WILL BE THE TIME PERIOD AFTER THE SITE HAS BEEN STRIPPED AND NOT YET RESTORED, BUILT UPON, OR PAVED. THE CONTRACTOR MUST INSTALL OR CONSTRUCT APPROPRIATE TEMPORARY SOIL EROSION AND SEDIMENTATION CONTROL MEASURES TO PROTECT ADJACENT PROPERTIES.
- 4. CONTRACTOR IS MADE AWARE THAT ANY PAVEMENT WORK WITHIN AND 5 FEET SURROUNDING ADA PARKING AREAS SHALL HAVE A MAXIMUM SLOPE OF 2 PERCENT IN ANY DIRECTION. THE ENGINEER WILL INDICATE THE LOCATION OF THESE AREAS ON THE PLANS WHICH MUST BE CORRECTED AS PART OF THE NEW RESURFACING PROJECT.

PAVING CONSTRUCTION NOTES

- 1. PAVING CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE LOCAL, STATE DEPARTMENT OF TRANSPORTATION, AND/OR OTHER PERMITTING AGENCIES HAVING JURISDICTION AS APPLICABLE. WHERE THESE PLANS DIFFER FROM THE STANDARD DETAILS OR SPECIFICATIONS OF ANY GOVERNING AGENCY, THE AGENCY'S REQUIREMENTS SHALL GOVERN.
- 2. REMOVE ANY EXISTING TOPSOIL, VEGETATION, TREES AND OTHER DELETERIOUS MATERIALS TO EXPOSE THE SUBGRADE SOIL. TREE ROOTS SHALL BE COMPLETELY REMOVED.
- 3. EXCAVATE TO THE DEPTH OF THE FINAL SUBGRADE ELEVATION TO ALLOW FOR GRADE CHANGES AND THE PLACEMENT OF THE RECOMMENDED PAVEMENT SYSTEM.
- 4. WHERE FULL-DEPTH PAVEMENT RESTORATION IS CALLED FOR ON THE PLANS, THE TOP 12 INCHES OF THE EXPOSED SUBGRADE SHALL BE COMPACTED TO A DENSITY NO LESS THAN 95 PERCENT OF THE MAXIMUM DRY DENSITY AS DETERMINED BY THE MODIFIED PROCTOR (ASTM D 1557-91).
- 5. WHERE FULL-DEPTH PAVEMENT RESTORATION IS CALLED FOR ON THE PLANS, THE FINAL SUBGRADE SHALL BE THOROUGHLY PROOF ROLLED UNDER THE OBSERVATION OF THE MATERIALS TESTING AGENCY. LOOSE OR YIELDING AREAS WHICH CANNOT BE MECHANICALLY STABILIZED SHALL BE REMOVED AND REPLACED WITH ENGINEERED FILL OR AS DICTATED BY FIELD CONDITIONS UNDER THE APPROVAL OF THE ENGINEER AND/OR OWNER.
- 6. WHERE PARTIAL-DEPTH PAVEMENT RESTORATION IS CALLED FOR ON THE PLANS, THE AGGREGATE BASE SHALL BE COMPACTED TO A DENSITY NO LESS THAN 95 PERCENT OF THE MAXIMUM DRY DENSITY AS DETERMINED BY THE MODIFIED PROCTOR (ASTM D 1557-91). THE BASE SHALL EXTEND A MINIMUM OF 1 FOOT BEYOND THE PAVED EDGE.
- 7. CONSTRUCTION TRAFFIC SHALL BE MINIMIZED ON THE NEW PAVEMENT. IF CONSTRUCTION TRAFFIC IS ANTICIPATED ON THE PAVEMENT STRUCTURE, THE PLACEMENT OF THE FINAL LIFT SHALL BE DELAYED UNTIL THE MAJORITY OF THE CONSTRUCTION ACTIVITIES HAVE BEEN COMPLETED. THIS ACTION WILL ALLOW REPAIR OF LOCALIZED FAILURE, IF ANY DOES OCCUR, AS WELL AS REDUCE LOAD DAMAGE ON THE PAVEMENT SYSTEM. THE CONTRACTOR IS RESPONSIBLE FOR REPAIR TO ANY DAMAGED SECTION RESULTING FROM CONSTRUCTION ACTIVITY AT THE SOLE EXPENSE OF THE CONTRACTOR.
- 8. ANY CONCRETE CURB DISTURBED AND/OR CALLED OUT TO BE REPLACED DURING CONSTRUCTION SHOULD BE REPLACED TO MATCH THE EXISTING PROFILE OF THE CURB AND GUTTER. CONTRACTOR SHALL REVERSE GUTTER PANS, WHERE REQUIRED, TO PREVENT WATER PONDING AND/OR ENTRAPMENT AGAINST THE CURB.
- 9. WHERE A PROPOSED CURB AND GUTTER SECTION IS ADJACENT TO AN ADA RAMP, DROP CURB HEIGHT TO MAXIMUM 1/4" ACROSS THE RAMP OPENING.
- 10. PLACE EXPANSION JOINT AND JOINT SEALANT WHERE NEW CONCRETE PAVEMENT OR WALKS ABUT BUILDING WALLS (PROPOSED OR EXISTING), CURB, OR EXISTING CONCRETE PAVEMENT.
- 11. ANY CUT AND PATCH AREAS SHOWN ON THESE PLANS ARE APPROXIMATE IN SIZE AND LOCATION AND MAY BE ADJUSTED AT THE TIME OF CONSTRUCTION. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO FIELD VERIFY CUT AND PATCH AREAS SHOWN AND COMPLETE THE ENTIRE SCOPE OF WORK AT NO INCREASE IN LUMP SUM PRICE.
- 12. THE CONTRACTOR SHALL REMOVE EXISTING PAVEMENT IN A MANNER WHICH DOES NOT ADVERSELY AFFECT THE SURROUNDING AREAS OF THE SITE. ANY NEEDED REPAIRS DUE TO DAMAGE DONE DURING REMOVAL OPERATIONS TO SURROUNDING AREAS SHALL BE AT THE SOLE EXPENSE OF THE CONTRACTOR AND TO THE SATISFACTION OF THE OWNER.
- 13. THE SUBGRADE SHALL BE INSPECTED BY THE MATERIALS TESTING AGENCY AFTER PAVEMENT REMOVAL AND PRIOR TO REPLACEMENT. SUBGRADE UNDERCUTTING SHALL BE PERFORMED AT THE REQUEST OF THE TESTING AGENCY, OWNER, OR ENGINEER AND WILL BE PAID AS A SEPARATE ITEM, UNLESS SPECIFICALLY CALLED OUT FOR ON THE PLANS.
- 14. WHERE SECTIONS OF EXISTING PARKING LOTS AND/OR INTERNAL ROADWAYS ARE TO BE MODIFIED, THE CONTRACTOR SHALL PROVIDE ALL FINISHED PAVEMENT MARKINGS, INCLUDING INFORMATIONAL AND DIRECTIONAL MARKINGS, SUCH THAT THEY MATCH THE EXISTING MARKING TYPE, COLOR, PATTERN AND LOCATION. THE CONTRACTOR MUST TAKE FIELD MEASUREMENTS AND ESTABLISH CONTROLS IN THE FIELD PRIOR TO REMOVING OR COVERING THE EXISTING MARKINGS IN ORDER TO REPLACE THEM. NOT ALL EXISTING PAVEMENT MARKINGS THAT ARE TO BE RE-ESTABLISHED MAY BE SHOWN ON THE PROJECT PLANS. REFER TO THE PROJECT PLANS FOR AREAS OF PAVEMENT MARKINGS THAT ARE TO BE MODIFIED FROM THE EXISTING LAYOUT. CONTRACTOR SHALL INSTALL NEW PAVEMENT MARKINGS IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS. NOTIFY THE ENGINEER PRIOR TO NEW MARKINGS BEING PLACED IF ANY CONFLICTS ARE FOUND WITH THE EXISTING MARKINGS. CONTRACTOR SHALL INCLUDE COSTS TO PROVIDE ALL MARKINGS.
- 15. WHERE AREAS OF PARKING LOTS WILL BE REOPENED FOR CUSTOMER USE PRIOR TO FINAL PAVEMENT MARKINGS BEING INSTALLED, THE CONTRACTOR SHALL PROVIDE TEMPORARY STRIPING FOR THE CENTERLINE OF THE PARKING BAY IN ORDER TO ASSIST DRIVERS TO PARK IN THE INTENDED ALIGNMENT ALONG THE BAY.

NIGHT TIME WORK

- 1. ANY NIGHT TIME OR AFTER DARK WORK SHALL BE APPROVED BY THE OWNER AND LOCAL AGENCIES HAVING JURISDICTION.
- 2. CONTRACTOR SHALL SUPPLY AMPLE LIGHTING TO PERFORM THE NIGHT TIME WORK. LIGHTING SHALL BE SHIELDED FOR ADJACENT PROPERTIES AND ROADWAYS.
- 3. ALL NIGHT TIME WORK SHALL BE SCHEDULED 24 HOURS IN ADVANCE WITH THE OWNER.

DEMOLITION NOTES

- 1. THE CONTRACTOR SHALL PROTECT EXISTING WALKS, PAVEMENT, CURBS, GUTTERS, WALLS, FENCES, SIGNS, GATES, LANDSCAPING AND TREES TO REMAIN DURING CONSTRUCTION.
- 2. THE CONTRACTOR SHALL FIELD-VERIFY THE LOCATION OF ALL UTILITIES ON SITE PRIOR TO ANY CONSTRUCTION WORK. DAMAGE TO ANY UTILITIES DURING CONSTRUCTION SHALL BE REPAIRED AT THE SOLE EXPENSE OF THE CONTRACTOR.
- 3. THE CONTRACTOR SHALL STAGE CONSTRUCTION OPERATIONS SO AS TO NOT IMPEDE FACILITY ACCESS AND OPERATIONS.
- 4. DISPOSAL OF MATERIALS REMOVED FROM THE SITE IS THE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL DISPOSE OF ALL MATERIALS IN A TIMELY MANNER AND IN ACCORDANCE WITH ALL GOVERNING AGENCY REGULATIONS. BURYING DEBRIS ON THE SUBJECT SITE IS PROHIBITED.
- 5. EQUIPMENT AND MATERIALS USED IN CONSTRUCTION OPERATIONS SHALL BE STORED AND STOCKPILED IN ACCORDANCE WITH ALL GOVERNING AGENCY REGULATIONS AND AT THE DIRECTION OF THE OWNER OR THE ENGINEER.
- 6. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO MAINTAIN THE CLEANLINESS OF THE SITE THROUGHOUT THE PROJECT. THE CONTRACTOR SHALL CLEAN UP ANY DEBRIS ON SITE RESULTING FROM CONSTRUCTION OPERATIONS.
- 7. THE USE OF EXPLOSIVES SHALL NOT BE PERMITTED.
- 8. ALL NECESSARY RESTORATION WORK SHALL BE COMPLETED AT THE SOLE EXPENSE OF THE CONTRACTOR.
- 9. THE CONTRACTOR SHALL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. THIS REQUIREMENT WILL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS. ANY CONSTRUCTION OBSERVATION BY THE ENGINEER OF THE CONTRACTOR'S PERFORMANCE IS NOT INTENDED TO INCLUDE REVIEW OF THE ADEQUACY OF THE CONTRACTOR'S SAFETY MEASURES IN, ON OR NEAR THE CONSTRUCTION SITE. ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH CURRENT O.S.H.A. CODES AND STANDARDS. NOTHING INDICATED ON THESE PLANS SHALL RELIEVE THE CONTRACTOR FROM COMPLYING WITH APPROPRIATE SAFETY REGULATIONS.

SOIL EROSION AND SEDIMENTATION CONTROL NOTES

- 1. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL SOIL EROSION AND SEDIMENTATION CONTROL MEASURES IN ACCORDANCE WITH THE REQUIREMENTS OF ALL GOVERNING AGENCIES' ENVIRONMENTAL STANDARDS.
- 2. CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO KEEP ON-SITE SOIL EROSION SEDIMENT FROM ENTERING ADJACENT PARKING AREA AND INSURE THAT THE PARKING AREA IS KEPT CLEAN.
- 3. ALL SOIL EROSION AND SEDIMENTATION CONTROL MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE AND PRIOR TO ANY LAND DISTURBANCE AND/OR DEMOLITION.
- 4. ALL SOIL EROSION AND SEDIMENTATION CONTROL MEASURES ARE TO BE INSPECTED A MINIMUM OF ONCE PER WEEK AND/OR WITHIN 24 HOURS AFTER A RAIN EVENT. ALL SOIL EROSION AND SEDIMENTATION CONTROL MEASURES ARE TO BE MAINTAINED AND REPAIRED IMMEDIATELY AS NECESSARY.
- 5. SOIL EROSION AND SEDIMENTATION CONTROL MEASURES MAY HAVE TO BE ALTERED AT VARIOUS STAGES DURING CONSTRUCTION TO ACCOMMODATE ACTUAL SITE CONDITIONS.
- 6. THE CONTRACTOR IS RESPONSIBLE FOR CLEANING AND REMOVING ANY MUD/DEBRIS TRACKING FROM THE STREETS AND SIDEWALKS DURING THE SAME DAY OF OCCURRENCE.
- 7. ALL WASH WATER (PAVING TRUCKS, VEHICLE CLEANING, EQUIPMENT CLEANING, ETC.) SHALL BE DISPOSED OF IN AN APPROPRIATE MANNER THAT PREVENTS CONTACT BETWEEN THESE MATERIALS AND STORM WATER THAT IS DISCHARGED FROM THE SITE.
- 8. RUBBISH, TRASH, GARBAGE, LITTER OR OTHER SUCH MATERIALS SHALL BE DEPOSITED INTO SEALED CONTAINERS. MATERIALS SHALL BE PREVENTED FROM LEAVING THE PREMISES THROUGH THE ACTION OF WIND OR STORMWATER DISCHARGE.
- 9. SUFFICIENT OIL AND GREASE ABSORBING MATERIALS AND FLOTATION BOOMS SHALL BE MAINTAINED ON SITE OR READILY AVAILABLE TO CONTAIN AND CLEAN-UP FUEL OR CHEMICAL SPILLS AND LEAKS.
- 10. DUST ON THE SITE SHALL BE CONTROLLED BY SPRAYING WATER ON DRY AREAS OF THE SITE OR BY OTHER APPROVED MEANS THROUGHOUT ALL CONSTRUCTION OPERATIONS. WATERING TRUCKS SHALL BE USED AS NEEDED. THE USE OF MOTOR OILS AND OTHER PETROLEUM BASED OR TOXIC LIQUIDS FOR DUST SUPPRESSION OPERATIONS IS PROHIBITED.
- 11. THE CONTRACTOR SHALL FOLLOW ALL LOCAL AND STATE SOIL EROSION ORDINANCES TO INSURE THAT NO ON-SITE SOIL EROSION SEDIMENT LEAVES THE CONSTRUCTION SITE. ANY SOIL EROSION SEDIMENT OR CONSTRUCTION DEBRIS WHICH IS FOUND OFF-SITE SHALL BE PROMPTLY REMOVED AT CONTRACTOR'S SOLE EXPENSE.

RESTORATION NOTE

- 1. RESTORE ALL NON-PAVED AREAS WITH 3" OF CLEAN TOPSOIL AND SPECIFIED SEED MIX. PLACE MULCH IN ALL SEEDED AREAS. ON SLOPES IN EXCESS OF 10 HORIZONTAL TO 1 VERTICAL PLACE NORTH AMERICAN GREEN DS150 MULCH BLANKET IMMEDIATELY AFTER SEEDING. USE METAL STAPLES PER MANUFACTURERS RECOMMENDATIONS TO HOLD MATTING IN PLACE.
- 2. RESTORE ALL CONCRETE SIDEWALKS, ADA RAMPS AND/OR OTHER PEDESTRIAN ACCESS ITEMS SO THAT ACCESS IS NOT RESTRICTED FOR MORE THAN 24 HOURS.
- 3. CONTRACTOR SHALL PROVIDE TEMPORARY ADA PARKING STALLS, RAMPS, AND/OR OTHER ACCESSIBLE ROUTES AT ALL TIMES THAT PERMANENT ADA ITEMS ARE RESTRICTED.

07/07/2014
EROSION CONTROL
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NO.
REVISIONS
DATE
BY

Kimley»Horn
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KHA PROJECT	168719048
DATE	04/04/2020
SCALE	AS SHOWN
DESIGNED BY	ATO
DRAWN BY	ATO
CHECKED BY	JHP

GENERAL NOTES

**THE CENTER AT
HOBBS BROOK
STURBRIDGE, MA**